



## Planning and Transportation Committee

**Date:** TUESDAY, 23 JUNE 2015  
**Time:** 10.30 am  
**Venue:** LIVERY HALL - GUILDHALL

**Members:**

Michael Welbank (Chairman)	Christopher Hayward
Marianne Fredericks (Deputy Chairman)	Gregory Jones QC
Oliver Lodge	Deputy Henry Jones
Randall Anderson	Alderman Professor Michael Mainelli
Alex Bain-Stewart	Paul Martinelli
David Bradshaw	Brian Mooney
Dennis Cotgrove	Deputy Alastair Moss
Revd Dr Martin Dudley	Sylvia Moys
Peter Dunphy	Graham Packham
Emma Edhem	Judith Pleasance
Alderman Peter Estlin	Deputy Henry Pollard
Deputy Kevin Everett	Alderman William Russell
Sophie Fernandes	Tom Sleigh
Deputy Bill Fraser	Graeme Smith
George Gillon	Angela Starling
Alderman Timothy Hailes	Patrick Streeter
Deputy Brian Harris	Deputy James Thomson

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Lunch will be served in Guildhall Club at 1PM

John Barradell  
Town Clerk and Chief Executive

# **AGENDA**

## **Part 1 - Public Agenda**

1. **APOLOGIES**
2. **MEMBERS' DECLARATIONS UNDER THE CODE OF CONDUCT IN RESPECT OF ITEMS ON THE AGENDA**
3. **MINUTES**  
To agree the public minutes and summary of the meeting held on 2 June 2015.  
**For Decision**  
(Pages 1 - 8)
4. **TOWN PLANNING AND DEVELOPMENT APPLICATIONS**  
Report of the Chief Planning Officer and Development Director.  
**For Information**  
(Pages 9 - 22)
5. **VALID APPLICATIONS LIST FOR COMMITTEE**  
Report of the Chief Planning Officer and Development Director.  
**For Information**  
(Pages 23 - 26)
6. **REPORTS RELATIVE TO PLANNING APPLICATIONS**
  - a) Walsingham House, 35 Seething Lane, London EC3N 4AH  
**For Decision**  
(Pages 27 - 76)
  - b) Opposite Rising Sun Court Long Lane, London, EC1A 9AJ  
**For Decision**  
(Pages 77 - 94)
7. **REPORTS OF THE DIRECTOR OF THE BUILT ENVIRONMENT**
  - a) Adoption of the City of London Thames Strategy Supplementary Planning Document  
**For Decision**  
(Pages 95 - 176)
  - b) Transport for London Grants for the 2015/16 Financial Year (Joint report with the Director of Markets and Consumer Protection)  
**For Decision**  
(Pages 177 – 186)

8. **CITY DEVELOPMENT TRENDS UPDATE: JUNE 2015**  
Director of the Built Environment & City Property Advisory Team.  
**For Information**  
(Pages 187 - 192)
9. **CITY FUND HIGHWAY DECLARATION - HOTEL DEVELOPMENT AT 100 MINORIES EC3**  
Report of the City Surveyor.  
**For Decision**  
(Pages 193 - 200)
10. **CONTEXT AND UPDATE SESSIONS**  
Report of the Town Clerk.  
**For Information**  
(Pages 201 - 202)
11. **QUESTIONS ON MATTERS RELATING TO THE WORK OF THE COMMITTEE**
12. **ANY OTHER BUSINESS THAT THE CHAIRMAN CONSIDERS URGENT**
13. **EXCLUSION OF THE PUBLIC**  
MOTION – That under Section 100(A) of the Local Government Act 1972, the public be excluded from the meeting for the following items on the grounds that they involve the likely disclosure of exempt information as defined in Part I of Schedule 12A of the Local Government Act.

**For Decision**

## **Part 2 - Non-public Agenda**

14. **NON-PUBLIC MINUTES**  
To agree the non-public minutes of the meeting held on 2 June 2015.  
**For Decision**  
(Pages 203 - 204)
15. **RESTRUCTURE OF CITY TRANSPORTATION**  
Report of the Director of the Built Environment.  
**For Information**  
(Pages 205 - 214)
16. **NON-PUBLIC QUESTIONS ON MATTERS RELATING TO THE WORK OF THE COMMITTEE**
17. **ANY OTHER BUSINESS THAT THE CHAIRMAN CONSIDERS URGENT AND WHICH THE COMMITTEE AGREES SHOULD BE CONSIDERED WHILST THE PUBLIC ARE EXCLUDED**

**Any drawings and details of materials submitted for approval will be available for inspection by Members in the Livery Hall from Approximately 9:30 a.m.**

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## **PLANNING AND TRANSPORTATION COMMITTEE**

**Tuesday, 2 June 2015**

**Minutes of the meeting of the Planning and Transportation Committee held at the Guildhall EC2 at 10.30 am**

### **Present**

#### **Members:**

Michael Welbank (Chairman)	Sylvia Moys
Marianne Fredericks (Deputy Chairman)	Graham Packham
Randall Anderson	Judith Pleasance
Alex Bain-Stewart	Deputy Henry Pollard
Deputy Bill Fraser	Alderman William Russell
George Gillon	Graeme Smith
Deputy Brian Harris	Angela Starling
Gregory Jones QC	Patrick Streeter
Oliver Lodge	

#### **In attendance**

Jeremy Simons

#### **Officers:**

Simon Murrells	Assistant Town Clerk
Christopher Braithwaite	Town Clerk's Department
Natasha Dogra	Town Clerk's Department
Richard Chamberlain	City Surveyor's Department
Alison Hurley	City Surveyor's Department
Deborah Cluett	Comptroller and City Solicitor's Department
Philip Everett	Director of the Built Environment
Gemma Delves	Department of the Built Environment
Annie Hampson	Department of the Built Environment
Elisabeth Hannah	Department of the Built Environment
Stuart McGregor	Department of the Built Environment
Richard Miller	Department of the Built Environment
Paul Monaghan	Department of the Built Environment
Steve Presland	Department of the Built Environment
Alan Rickwood	City of London Police

Before commencing the business of the meeting, the Chairman invited Jeremy Simons, immediate past Chairman of the Streets and Walkways Sub-Committee to provide Members with a presentation regarding the improvements that had been made to the Holborn Circus area and the restoration of the Prince Albert statue in the area.

The Chairman also reminded Members that following the meeting they would be provided with a presentation in relation to the Consideration Contractor Scheme.

1. **APOLOGIES**

Apologies for absence were received from Reverend Dr Martin Dudley, Emma Edhem, Alderman Peter Estlin, Sophie Anne Fernandes, Christopher Hayward, Deputy Henry Jones, Paul Martinelli, Brian Mooney, Deputy Alastair Moss, Tom Sleigh and Deputy James Thomson.

2. **MEMBERS' DECLARATIONS UNDER THE CODE OF CONDUCT IN RESPECT OF ITEMS ON THE AGENDA**

There were no declarations of interest.

3. **MINUTES**

**RESOLVED** – That the minutes of the previous meeting held on 12 May 2015 were approved as an accurate record, subject to the following inclusions:

- The inclusion following the final sentence of minute item 10.2 of the sentence “There was also a concern that the proposal had not changed since referral back in regard to the mass of the structure. The concerns stated by the Committee in this area had not been addressed.”

Matters Arising

**Cycle Superhighway** - The Director of the Built Environment the Committee that the Chairman had written on their behalf to Transport for London in relation to this scheme.

**Sugar Quay** - The Chairman advised the Committee that an appeal had been lodged in relation to the modification of the Section 106 agreement for Sugar Quay.

4. **TOWN PLANNING AND DEVELOPMENT APPLICATIONS**

The Committee received a report of the Chief Planning Officer and Development Director relative to development and advertisement applications dealt with under delegated authority since the last meeting.

5. **VALID APPLICATIONS LIST FOR COMMITTEE**

The Committee received a report of the Chief Planning Officer and Development Director which provided details of valid planning applications received by the department.

6. **REPORTS RELATIVE TO PLANNING APPLICATIONS**

The Chief Planning Officer advised the Committee that there were a number of common areas between the applications as follows details of which were contained within the reports.

**6.1 2no. BT Telephone Kiosks Royal Exchange Buildings, London, EC3V 3NL**

The Chief Planning officer explained that there were concerns that the proposed change of use would detract from the amenities of the Grade 1 and 2\* listed buildings in the area around the Royal Exchange and the other iconic structures in the Bank Conservation Area and would lead to clutter on the street scene and congestion among pedestrians in an already very busy area.

During discussion, reference was made to non-designated heritage assets which were not listed buildings or conservation areas, but were considered to have heritage value.

A Member asked what would happen to these and similar phone boxes in the long term if alternative uses were not found and regular maintenance not conducted. It was explained that this issue was not a valid planning consideration for these applications, but a report would be submitted to the Committee on this issue in due course.

Upon being put to the vote the application was refused –

Vote: 13 in support, 1 abstention.

**RESOLVED** – That the application be refused for the reasons set out in the attached schedule (namely that the proposed conversion of the telephone boxes to retail units (Use Class A1) would detract from the significance of the K6 telephone boxes and would result in less than substantial harm to part of the Bank Conservation Area and the setting of the grade I listed Royal Exchange and grade II listed Royal Exchange Buildings. The proposal would detract from the public realm and obstruct the highway contrary to policies DM17.1, DM12.2, DM12.1, DM10.1 and DM10.4 of the Local Plan and policies 6.10B, 7.5B and 7.8 of the London Plan.).

**6.2 2 No. BT Telephone Kiosks On Finsbury Circus At Side of 88 - 92 Moorgate London EC2M 6SE**

**Proposal:** Change of use of 2no. BT telephone boxes to 2no. retail kiosks (A1). Replacement of the existing telephone box glazing with toughened safety glass.

**Registered No:** 15/00039/FULL

The Chief Planning officer explained that there were concerns that the proposed change of use would detract from the Finsbury Circus Conservation Area and would lead to clutter on the streetscene and congestion among pedestrians in what would be an extremely busy area following the opening of the nearby Crossrail station and that there was already significant amounts of street furniture adjacent to the site.

Upon being put to the vote the application was refused –

Vote: 15 in support, 1 abstention.

**RESOLVED** – That the application be refused for the reasons set out in the attached schedule (namely that the proposed conversion of the telephone boxes to retail units (Use Class A1) would detract from the significance of the K6 telephone boxes and result in less than substantial harm to part of the Finsbury Circus Conservation Area and the setting of the grade II listed 76 to 92 Moorgate. The scheme would obstruct the highway and would detract from the public realm contrary to policies DM17.1, DM12.2, DM12.1, DM10.1 and DM10.4 of the Local Plan and policies 6.10B, 7.5B and 7.8 of the London Plan.).

**6.3 1 No. BT Telephone Kiosk O/s 21 Fleet Street, London, EC4Y 1AA**  
**Proposal:** Change of use of 1 no. BT telephone boxes to 1 no. retail kiosk (A1). Replacement of the existing telephone box glazing with toughened safety glass.

**Registered No:** 15/00041/FULL

The Chief Planning officer explained that there were concerns that the proposed change of use would detract from the Fleet Street Conservation Area and the heritage assets nearby, and would lead to clutter on the streetscene and congestion among pedestrians in what was already a busy area.

Upon being put to the vote the application was refused –

Vote: 15 in support, 1 abstention.

**RESOLVED** – That the application be refused for the reasons set out in the attached schedule (namely that the proposed conversion of the telephone box to a retail unit (Use Class A1) would detract from the significance of the K6 telephone boxes and result in less than substantial harm to part of the Fleet Street Conservation Area and the setting of the grade II listed 18 and 21 Fleet Street. The scheme would obstruct the highway and would detract from the public realm contrary to policies DM17.1, DM12.2, DM12.1, DM10.1 and DM10.4 of the Local Plan and policies 6.10B, 7.5B and 7.8 of the London Plan.).

**6.4 1 No. BT Telephone Kiosk O/s Bank Buildings 8 Lothbury London EC2R 7HH**

**Proposal:** Change of use of 1 no. BT telephone box to 1 no. retail kiosk (A1). Replacement of the existing telephone box glazing with toughened safety glass.

**Registered No:** 15/00042/FULL

The Chief Planning officer explained that there were concerns that the proposed change of use would detract from the Bank Conservation Area and the heritage assets nearby, and would lead to large amounts of clutter on the street scene and congestion among pedestrians in what was already a busy area.

A Member commented on the security of access for the site.

Upon being put to the vote the application was refused –

Vote: 15 in support, 1 abstention.

**RESOLVED** – That the application be refused for the reasons set out in the attached schedule (namely that the proposed conversion of the telephone box to a retail unit (Use Class A1) would detract from the significance of the K6 telephone box and result in less than substantial harm to part of the Bank Conservation Area. The scheme would obstruct the highway and detract from the public realm contrary to policies DM17.1, DM12.2, DM12.1, DM10.1 and DM10.4 of the Local Plan and policies 6.10B, 7.5B and 7.8 of the London Plan.

## **7. REPORTS OF THE DIRECTOR OF THE BUILT ENVIRONMENT**

### **7.1 Rescission of City Walkway - Moorfields Highwalk - 21 Moorfields Redevelopment**

The Committee considered a report of the Director of the Built Environment which recommended the rescission of part of the existing city walkway known as Moorfields Highwalk and seeks authority to enter into Section 106 covenants to safeguard its re-provision. The walkway rescission is to enable the redevelopment of 21 Moorfields which was approved by the Committee on 17 March 2015, subject to a Section 106 Agreement (Registered No. 14/01179/FULEIA).

Members asked questions about ensuring spares for the remaining lift, the proposal that the action was taken before the consultation notice expired and why the current City walkway would become private realm.

The Director of the Built Environment explained that the main purpose of the existing walkway was to provide access to the premises at 21 Moorfields, and therefore it was in the interests of the premises owner to maintain this walkway. He clarified that the notice that was notice of the action being taken, and the consultation had already been completed. The City Surveyor explained that the list of critical spares for the remaining lift was being compiled.

**RESOLVED** – That:

- a) authority be delegated to the Chief Planning Officer to agree the final terms of the proposed Section 106 covenants relating to the re-provision of city walkway as part of the 21 Moorfields development;
- b) subject to the Section 106 covenants referred to above and outlined under the “Section 106 Covenants” heading in the report, the resolution of the Court of Common Council dated 26 May 1977 be varied to alter the city walkway known as Moorfields Highwalk so as to exclude the area shown cross hatched on the City Walkway Variation Drawing No. M000040-HB-010 (Appendix D to this report) in accordance with the resolution set out in Appendix C to the report;
- c) the Director of the Built Environment be authorised to insert an appropriate date for the coming into force of the resolution at (b) above; and

- d) the proposed temporary closure under delegated authority prior to the City Walkway rescission coming into effect be noted.

## **7.2 Gateway 3 Outline Options Appraisal: Tower Bridge Bascule Re-Decking and Approach Viaduct Waterproofing**

The Committee considered a report of the Director of the Built Environment which set out the options appraisal for the Tower Bridge Bascule re-decking and approach viaduct waterproofing.

### **RESOLVED – That:**

- a) the Director of Built Environment be authorised to progress further investigations, in order to provide a firm recommendation on the preferred construction option at Gateway 4 to reduce construction risks;
- b) the Director of Built Environment be authorised to instruct AECOM in a limited initial role, to investigate practical outline solutions to the works, assist in the brief/tender for a Design & Build (D&B) contractor, as well as specifying and supervising exploratory works to inform the GW4 recommendations, the subsequent design proposals and to reduce construction stage risks – working in conjunction with the design and build contractor when appointed. This will be on hourly rates as “additional services” under their current term contract, capped at an estimated value of £62,000 to Gateway 4;
- c) the Director of Built Environment be authorised to tender and appoint a Cost Consultant for the duration of the project (with estimated costs to GW4 as £39,000);
- d) the Director of Built Environment be authorised to tender and appoint a Design and Build Contractor, with an initial appointment in an ECI role to GW4, estimated at £47,000;
- e) the Director of Built Environment be authorised to instruct intrusive investigation works to be carried out, to inform the design, estimated at £50,000 to GW4;
- f) the Director of Built Environment be authorised to allocate staff resources to an estimated value of £25,000, to progress the project to GW4 and
- g) approval be given to allocate resources to the value of £223,000 to GW4 in Financial Year 2015/16 to this project, to cover the costs of consultant fees, investigations and staff costs (the breakdown for which is Appended to this report). The cost of these resources will be covered by re-allocating from the sum of £459,000 in the current plan for 2015/16 for re-tensioning the suspension cables to Millennium Bridge, which are not required this financial year.

## **7.3 Parking Ticket Office Update and Annual Statistics for 2013-2015**

The Committee considered a report of the Director of the Built Environment which advised Members on the activities and progress of the Parking Ticket Office. The report set out the key successes and improvements over what has been a highly successful year, particularly in relation to quality, efficiency and contract savings. It also set out the key business challenges for the current year.

A Member explained that the City's parking hours were not in sync with those of neighbouring authorities and asked whether there were any plans for these to be amended. It was explained that this was a strategic issue that was not within the direct responsibility of the Parking Ticket Office, but further consideration could be given to this by the Department of the Built Environment.

A Member asked whether there were any plans to review parking arrangements in light of the proposals for Cultural Hub. It was explained that this was an issue which was discussed as part of the cross-departmental planning processes.

**RESOLVED** – That the Committee notes the report.

8. **QUESTIONS ON MATTERS RELATING TO THE WORK OF THE COMMITTEE**

There were no questions.

9. **ANY OTHER BUSINESS THAT THE CHAIRMAN CONSIDERS URGENT**

There were no items of urgent business.

10. **EXCLUSION OF THE PUBLIC**

**RESOLVED** – That under Section 100A(4) of the Local Government Act 1972, the public be excluded from the meeting for the following items of business on the grounds that they involved the likely disclosure of exempt information as defined in Part I of Schedule 12A of the Local Government Act.

11. **ISSUE REPORT: RELIGHTING OF TOWER BRIDGE 2012**

The Committee considered and approved a report of the City Surveyor regarding outstanding issues in relation to the Relighting of Tower Bridge which took place in 2012.

12. **NON-PUBLIC QUESTIONS ON MATTERS RELATING TO THE WORK OF THE COMMITTEE**

There were no questions.

13. **ANY OTHER BUSINESS THAT THE CHAIRMAN CONSIDERS URGENT AND WHICH THE COMMITTEE AGREES SHOULD BE CONSIDERED WHILST THE PUBLIC ARE EXCLUDED**

There were no items of urgent business.

**The meeting closed at 11.25 am**

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Chairman

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<b>Committee:</b>	<b>Date:</b>	
Planning and Transportation	23 <sup>rd</sup> June 2015	
<b>Subject:</b> Delegated decisions of the Chief Planning Officer and Development Director		
<b>Public</b>		

1. Pursuant to the instructions of your Committee, I attach for your information a list detailing development and advertisement applications determined by the Chief Planning Officer and Development Director or those so authorised under their delegated powers since my report to the last meeting.
2. Any questions of detail arising from these reports can be sent to [plans@cityoflondon.gov.uk](mailto:plans@cityoflondon.gov.uk).

## DETAILS OF DECISIONS

Registered Plan Number & Ward	Address	Proposal	Date of Decision
15/00365/LBC Aldersgate	102 John Trundle Court Barbican London EC2Y 8NE	Internal alterations including changes to layout and installation of partition walls.	02.06.2015
15/00354/PODC Aldersgate	2 Fann Street London EC2Y 8BR	Submission of Highway Schedule of Condition Survey pursuant to schedule 3, paragraph 6 of section 106 agreement dated 06 March 2015 planning permission reference 14/00322/FULMAJ.	21.05.2015
15/00119/FULL Aldersgate	1 Lauderdale Tower Barbican London EC2Y 8BY	Change of use of the ground floor retail unit from hairdressers (Class A1) to form a hairdressers and yoga studio (sui generis).	05.06.2015
15/00404/PODC Aldgate	60 & 70 St Mary Axe London EC3A 8JQ	Submission of details of the Highways Schedule of Condition pursuant to schedule 3, paragraph 11.1 of the Section 106	28.05.2015

		agreement signed in respect of planning permission ref 08/00739/FULEIA dated 10 June 2010.	
15/00357/PODC Aldgate	Dixon House 72 - 75 Fenchurch Street & 1 Lloyds Avenue London EC3M 4BR	Submission of details of Highways Schedule of Condition pursuant to paragraph 7.1 of Schedule 3 of the S106 Agreement signed in relation to planning permission 14/00579/FULL dated 25/02/2015	19.05.2015
15/00356/PODC Aldgate	Dixon House 72 - 75 Fenchurch Street & 1 Lloyds Avenue London EC3M 4BR	Submission of details of Local Procurement Strategy pursuant to paragraph 2.1 and Local Training Skills and Job Brokerage Strategy pursuant to paragraph 3.2 of Schedule 3 of the S106 Agreement signed in relation to planning permission 14/00579/FULL dated 25/02/2015.	04.06.2015
15/00348/MDC Aldgate	Dixon House 72 - 75 Fenchurch Street & 1 Lloyds Avenue London EC3M 4BR	Details of facilities and methods to accommodate construction vehicles and deliveries during demolition and construction pursuant to condition 2 of planning permission (application no. 14/00579/FULL) dated 25th February 2015.	04.06.2015
15/00336/FULLR3 Aldgate	30 St Mary Axe Plaza, South West Quadrant, London EC3A 8EP	Temporary installation of a sculpture, 'Red Atlas' by Ekkehard Altenburger, for a temporary period of one year to be taken down on or before 05 June 2016.	28.05.2015
15/00335/FULLR3 Aldgate	30 St Mary Axe Plaza South-East Quadrant London EC3A 8BF	Temporary installation of an artwork, 'Carson, Zezi, Emma, Takeshi, Nia' by Tomoaki Suzuki for a temporary period of up to one year to be taken down on or before 05 June 2016.	28.05.2015
15/00323/LBC Aldgate	Dixon House 72- 75 Fenchurch Street & 1 Lloyd's Avenue	Application under Section 19 of the Planning (Listed Buildings and Conservation Areas) Act 1990 to vary	26.05.2015

	London EC3N 3DH	condition 4 of listed building consent (application no. 14/00596/LBC) dated 25th February 2015 to refer to a revised list of drawings amended to reflect minor alterations to the detailed design.	
15/00313/NMA  Aldgate	Dixon House 72-75 Fenchurch Street 1 Lloyd's Avenue London EC3N 3DH	Non-Material Amendment under Section 96A of the Town and Country Planning Act 1990 to planning permission 14/00579/FULL dated 25th February 2015 to allow minor revisions to the internal floor levels at 5th and 6th floors together with lowering of the 6th floor dormer windows.	26.05.2015
15/00312/FULLR3  Aldgate	Outside 7 Bury Court London EC3A 8FR	Temporary installation of a sculpture, 'Organism of Control #8' by Keita Miyazaki, for a temporary period of up to one year, to be taken down on or before 05.06.2016.	28.05.2015
15/00271/MDC  Aldgate	60 St Mary Axe London EC3A 8JQ	Details of a demolition method statement pursuant to condition 9 of planning permission (application no. 08/00739/FULEIA) dated 10th June 2010.	26.05.2015
15/00214/MDC  Aldgate	60 - 70 St Mary Axe London EC3A 8JQ	Submission of details of Construction Logistics Plan and the Construction Routes Report pursuant to condition 8 of planning permission dated 10th June 2010 (Ref: 08/00739/FULEIA).	26.05.2015
14/00809/MDC  Bassishaw	Land Bounded By London Wall, Wood Street, St. Alphage Gardens, Fore Street, Fore Street Avenue, Bassishaw Highwalk, Alban Gate Rotunda, Alban Highwalk, Moorfields	Details of city walkway finish pursuant to condition 16 (a) (in part) of planning permission dated 30.06.2014 (ref: 14/00259/FULL).	21.05.2015

	Highwalk & Willoughby Highwalk, London, EC2		
15/00524/NMA Billingsgate	Sugar Quay Lower Thames Street London EC3R 6EA	Application under S96A of the Town and Country Planning Act 1990 (as amended) for a non material amendment to the wording of conditions 17(b) and 18(b) of planning permission 12/01104/FULMAJ dated 16.09.2013.	04.06.2015
15/00249/MDC Billingsgate	Sugar Quay Lower Thames Street London EC3R 6EA	Details of sewer vents pursuant to condition 15 of planning permission dated 16.09.13 (12/01104/FULMAJ).	02.06.2015
15/00495/MDC Bishopsgate	Alderman's House 117, 119, 121 Bishopsgate 34-37 Liverpool Street London EC2	Particulars and samples of Perryfield Whitbed Portland Stone pursuant condition 15 (a) (in part) of planning permission dated 28th March 2012(09/00192/FULMAJ) as amended by permission dated 23rd June 2014 (13/01199/FULMAJ).	04.06.2015
15/00351/ADVT Bishopsgate	15-25 Artillery Lane London E1 7LP	Installation of (i) one externally illuminated menu box measuring 0.54m high by 0.42m wide and 1.29m above ground level; (ii) one externally illuminated projecting sign measuring 0.68m high by 0.52m wide and 3.10m above ground level.	04.06.2015
15/00341/MDC Bishopsgate	Broadgate Circle 3 Broadgate London EC2M 2QS	Details of anti-vibration pads pursuant to condition 6 of Planning Permission 12/00431/FULL dated 24th July 2012.	04.06.2015
15/00340/MDC Bishopsgate	Broadgate Circle 3 Broadgate London	Details of an acoustic report pursuant to condition 4a of Planning Permission	04.06.2015

	EC2M 2QS	12/00431/FULL dated 24th July 2012.	
15/00334/FULLR3 Bishopsgate	St Botolph Without Bishopsgate Churchyard London EC3M 3TL	Temporary installation of a sculpture, 'Altar' by Kris Martin, for a temporary period of up to one year to be taken down on or before 05 June 2016.	04.06.2015
15/00330/LBC Bishopsgate	6 New Street London EC2M 4TP	Internal alterations and installation of smoke vents at roof level in connection with a change of use.	04.06.2015
15/00329/FULL Bishopsgate	6 New Street London EC2M 4TP	Change of use from a single dwelling house to 5no. self-contained studios flats and installation of smoke vents at roof level.	04.06.2015
15/00291/ADVT Bishopsgate	112-114 Middlesex Street & 1 Widegate Street London E1 7HY	Installation and display of (i) one internally illuminated projecting sign measuring 0.6m high, 0.6m wide, at a height above ground of 3.18m (ii) one internally illuminated menu box measuring 0.8m high, 0.3m wide, at a height above ground of 1.05m.	21.05.2015
15/00188/MDC Bishopsgate	Broadgate Circle & 3 Broadgate London EC2M 2QS	Details of canopies pursuant to condition 2(b) part of Planning Permission 12/00431/FULL dated 24th July 2012.	21.05.2015
15/00179/FULL Bishopsgate	16 - 17 Devonshire Square London EC2M 4SQ	Change of use of part ground, part lower, and part basement floors from Class B1(a) office use, to flexible Class A1 retail, A2 financial and professional services, A3 restaurants and cafes, and B1(a) office uses; rear and side infill extensions; light-well infill; formation of doorway in lieu of window within Devonshire Row elevation; formation of accessible entrance, doorway in lieu of window,	02.06.2015

		and fourth floor balcony within Devonshire Square elevation; and formation of roof level plant area.	
15/00136/MDC Bishopsgate	Alderman's House 117, 119, 121 Bishopsgate 34 - 37 Liverpool Street London EC2M 3TH	Details of the substation doors pursuant to condition 15 (b), (c), (f) and (i) (all in part) of planning permission dated 28th March 2012 (09/00192/FULMAJ) as amended by permission dated 23rd June 2014 (13/01199/FULMAJ).	19.05.2015
15/00079/MDC Bishopsgate	4 - 5 Devonshire Square London EC2M 4YD	Details of an archaeological watching brief pursuant to condition 2 of planning permission 14/00849/FULL dated 15 December 2014.	21.05.2015
15/00407/MDC Bridge And Bridge Without	2,3 & 5 Philpot Lane London EC3M 8AN	Submission of an Interim Travel Plan to discharge condition 7 pursuant to planning permission 12/00575/FULL dated 06.12.2012.	19.05.2015
15/00349/PODC Bridge And Bridge Without	20 Fenchurch Street London	Submission of details of Television and Radio Reception Impact Study pursuant to paragraph 6.1 of schedule 1 of the S106 Agreement signed in relation to planning permission reference 08/01061/FULMAJ	02.06.2015
14/01070/MDC Bridge And Bridge Without	33 King William Street London EC4R 9AS	Details of the provision Walkway stairs and a Walkway lift within the development pursuant to condition 12 of planning permission dated 17.01.2012 (11/00933/FULMAJ).	26.05.2015
15/00352/LBC Broad Street	27A Throgmorton Street London EC2N 2AN	Retention of fascia sign, paintwork and plaque, and installation of picture light above door and hanging sign to right hand side of main entrance.	02.06.2015
15/00084/ADVT Broad Street	27A Throgmorton Street London EC2N 2AN	Installation and display of (i) 1no externally illuminated fascia sign measuring	02.06.2015

		1.26m by 1.64m with lettering 0.23m high situated at a height above ground of 2.65m, (ii) 1no non-illuminated hanging sign measuring 0.45m by 0.45m situated at a height above ground of 2.75m (iii) 1no plaque sign measuring 0.3m by 0.38m situated at a height above ground of 1.36m.	
15/00051/MDC Broad Street	The Arbitrager 27A Throgmorton Street London EC2N 2AN	Details of the lobby floor grey sandstone paving, grille to the fan light, external doors in the lobby, and external security door pursuant to conditions 2 (a) and (b) of LBC dated 26/09/13 (Application no. 13/00785/LBC)	02.06.2015
15/00290/MDC Candlewick	Arthur Street London EC4R 9AY	Details of a scheme to protect nearby residents and commercial occupiers from noise, dust and other environmental effects and a Written Scheme of Investigation for an Archaeological Watching Brief pursuant to conditions 3 and 5 of planning permission dated 13th May 2015 (App No 14/01074/FULEIA).	21.05.2015
14/00939/MDC Candlewick	32 Lombard Street London EC3V 9BQ	Details of archaeological evaluation pursuant to Condition 7 of planning permission dated 17/12/2010 (application number 10/00344/FULL).	21.05.2015
15/00205/FULL Castle Baynard	Faraday Building 136 - 144A Queen Victoria Street London EC4V 4BU	Installation of one louvre at fourth floor level in place of the glazing in existing window on the eastern elevation.	19.05.2015
15/00200/MDC Castle Baynard	12 New Fetter Lane London EC4A 1AG	Submission of details of condition 2 pursuant to 11/00423/FULL (dated 18th August 2011).	21.05.2015

15/00270/MDC Coleman Street	30 - 34 Moorgate London EC2R 6DN	Details of a method statement for all foundations, basement and ground floor structure pursuant to condition 17 of planning permission 14/01134/FULL dated 8/1/2015 as amended by non-material amendment 15/00177/NMA dated 24/03/2015.	04.06.2015
14/00823/MDC Cordwainer	39-53 Cannon Street, 11-14 Bow Lane & Watling Court, London EC4.	Details of Sewer Vents pursuant to condition 7 of application dated 27th February 2014 reference 13/00339/FULMAJ.	21.05.2015
15/00389/LDC Cornhill	52 Threadneedle Street London EC2R 8AY	Details of "Cloud" feature lighting pursuant to condition 2(a) of listed building consent (application no. 14/01259/LBC) dated 7th April 2015.	21.05.2015
15/00216/FULL Cripplegate	Barbican Centre Sculpture Court Silk Street London EC2Y 8DS	Siting of two yurts and a projection screen on the Sculpture Court (adjacent to Frobisher Crescent and the Art Gallery) for a temporary period from 22nd June 2015 until 2nd August 2015 as part of the Barbican Art Gallery's 'Station to Station' exhibition.	22.05.2015
15/00213/FULL Cripplegate	Barbican Centre Lakeside Terrace Silk Street London EC2Y 8DS	Siting of two yurts, a thermochromic bench and two see-saws on the Lakeside Terrace for a temporary period from 22nd June 2015 until 2nd August 2015 as part of the Barbican Art Gallery's 'Station to Station' exhibition.	22.05.2015
15/00347/FULL Dowgate	Bank of China Building 90 Cannon Street London EC4N 6HA	Installation of three air conditioning units and acoustic screen on the 4th floor roof.	28.05.2015



15/00297/FULL Dowgate	Thames Tower, Princess House 1 Suffolk Lane London EC4R 0AX	Replacement of the fenestration and associated external alterations.	21.05.2015
15/00402/PODC Farringdon Within	20 Farringdon Street London EC4A 4AB	Submission of details of the Local Procurement Strategy pursuant to schedule 3, paragraph 5.1 of the Section 106 agreement signed in respect of planning permission ref 14/00266/FULMAJ dated 30 June 2014.	28.05.2015
15/00343/MDC Farringdon Within	3 - 4 Bartholomew Place London EC1A 7HH	Submission of a Demolition Statement, Scheme for protecting nearby residents and commercial occupiers from noise and dust, Details of samples and materials pursuant to conditions 2, 3 and 5 (part) of planning permission 12/00764/FULL dated 25/09/2012.	04.06.2015
15/00277/NMA Farringdon Within	Fleet Building, 40 Shoe Lane, 70 Farringdon Street, Plumtree Court, 42 Shoe Lane, 12 Plumtree Court And 57 Farringdon Street London EC4A 4AP	Application under Section 96a of the Town and Country Planning Act 1990 for non-material amendments to remove condition 2 (Tree Protection) and vary condition 9 (Construction and Logistics Plan) of planning permission 12/01225/FULEIA dated 28 October 2013.	19.05.2015
15/00027/MDC Farringdon Within	3 Cloth Street London EC1A 7LD	Particulars and samples, details of ground floor elevations, ground floor entrance, louvres, handrails and balustrades, alterations to the existing facades and junctions with adjoining premises pursuant to condition 2(a)(in part), (b)(in part), (c)(in part), (d), (g), (h)(in part) and (i) of planning permission dated 3rd July 2014 (ref:	21.05.2015

		14/00458/FULL).	
15/00442/NMA  Farringdon Without	1-6 Dyer's Buildings London EC1N 2JT	Application under section 96a of the Town and Country Planning Act 1990 for a non-material amendment to planning permission dated 1st July 2013 (ref: 11/00885/FULMAJ) to enable: (a) internal reconfiguration to the lower ground floor; (b) the relocation of a window; (c) the removal of roof lights; (d) amendment to the building line on the south elevation at first floor level; and (e) amendments to the window frame details on the south elevation.	21.05.2015
15/00301/LBC  Farringdon Without	St Bartholomew's Hospital West Smithfield Museum House London EC1A 7BE	Alterations to modernise existing second floor lecture theatre comprising: removal of the raked seating; general repairs and new finishes throughout; the installation of secondary glazing; the introduction of a new mechanical ventilation system including air handling units and new grilles.	04.06.2015
15/00266/FULL  Farringdon Without	1 Essex Court Middle Temple London EC4Y 9AR	Change of use of 1 no. residential dwelling at second floor level to office use (88sq.m).	04.06.2015
15/00248/LBC  Farringdon Without	10 Furnival Street London EC4A 1AB	Internal alterations at 1st floor level.	22.05.2015
15/00207/MDC  Farringdon Without	23 West Smithfield London EC1A 9HY	Submission of details of materials, windows, handrails and balustrades and the boundary treatment between 22 and 23 West Smithfield pursuant to the discharge of condition 2 parts a, b, c and d of	19.05.2015

		planning permission reference 13/01044/FULL dated 16th January 2014.	
15/00122/MDC Farringdon Without	St Bartholomews Hospital West Smithfield London EC1A 7BE	Details of the relocated cycle parking pursuant to the discharge of condition 10 of planning permission 14/01283/FULL dated 24th February 2015.	04.06.2015
15/00338/PODC Langbourn	Land Bounded By Fenchurch Street, Fen Court, Fenchurch Avenue And Billiter Street (120 Fenchurch Street), London EC3	Submission of details of Utility Connections Programme pursuant to paragraph 13.1 of Schedule 2 of the S106 Agreement signed in relation to planning permission 11/00854/FULEIA dated 30/03/2012.	28.05.2015
15/00278/MDC Langbourn	Land Bounded By Fenchurch Street, Fen Court, Fenchurch Avenue & Billiter Street (120 Fenchurch Street) London EC3	Details of rainwater harvesting and grey water recycling systems pursuant to condition 14 of planning permission dated 30/03/2012 (11/00854/FULEIA).	21.05.2015
15/00259/MDC Langbourn	Land Bounded By Fenchurch Street, Fen Court, Fenchurch Avenue & Billiter Street (120 Fenchurch Street) London EC3	Details of a borehole cooling system pursuant to condition 3 of planning permission dated 30/03/2012 (11/00854/FULEIA).	21.05.2015
15/00320/FULLR3 Lime Street	Outside St Helen's Square, Land Close To The Junction With St. Mary Axe & Leadenhall Street London EC3A 8EE	Temporary installation of a sculpture, 'O my friends there are no friends' by Sigalit Landau, for a temporary period of up to one year to be taken down on or before 05 June 2016.	28.05.2015

15/00319/FULL Lime Street	5 St Helen's Place London EC3A 6AU	Installation of a new entrance canopy to the Leathersellers' Hall main entrance and re-instatement of entrance gates.	21.05.2015
14/01208/MDC Portsoken	9 - 13 Aldgate High Street London EC3	Details of provision of sewer vents, and piling method statements to prevent potential for damage to the subsurface water and subsurface sewerage infrastructure pursuant to Conditions 10, 11 and 12 of planning permission dated 08.04.14 app.no. 13/00590/FULL	21.05.2015
15/00464/NMA Tower	76 - 86 Fenchurch Street, 1 - 7 Northumberland Alley & 1 & 1A Carlisle Avenue London EC3N 2ES	Non-Material Amendment under Section 96A of the Town and Country Planning Act 1990 to planning permission 08/00824/FULMAJ dated 11th November 2014 to vary the wording of condition 4 enabling demolition of the existing buildings prior to the submission of details required specifically by the condition.	04.06.2015
15/00296/FULL Tower	Station Forecourt Fenchurch Street Railway Station Fenchurch Place London EC3M 4AJ	Installation of a freestanding coffee kiosk (Class A1) with connections to utilities (water, electric and drainage).	19.05.2015
15/00276/ADVT Tower	Unit 2 Three Quays Walk London EC3R 6AH	Installation and display of six signs on canvas screens in association with an external seating area each measuring 0.2 metres high by 0.45 metres wide.	04.06.2015
15/00275/FULL Tower	2 Three Quays Walk London EC3R 6AH	Use of private land for retail purposes (Use Class A1) and the placing of 28 tables and 56 chairs ancillary to the adjoining retail use.	04.06.2015
15/00227/FULL Tower	Bakers Hall 7 Harp Lane London	Replacement of plant at roof level with a single storey roof extension. Installation	21.05.2015

	EC3R 6DP	of 10 new condenser units at ground floor level; refurbishment works to main entrance; replacement of the existing windows; reconfiguration of the basement to provide enhanced cycle storage including a new cycle access lane from Harp Lane.	
14/01168/FULL Tower	Unit 3 Three Quays Lower Thames Street London EC3R 6DT	Use of private land for retail purposes (Use Class A1) and the placing of 48 chairs, 32 tables, 8 benches and 6 planters ancillary to the adjoining Class A1 use.	28.05.2015
15/00074/MDC Vintry	40 Cannon Street London EC4N 6JJ	(i) Submission of samples of materials to be used on all external faces of the building, (ii) Details of: party walls, ground floor elevations, ground floor entrances, flank walls, soffits, handrails and balustrades and junctions with adjoining premises pursuant to condition 8 (PART) of planning permission dated 18.09.14 (14/00774/FULL)	02.06.2015
15/00202/ADVT Walbrook	The Walbrook Building 25 Walbrook London EC4N 8AF	Retention of (i) one internally illuminated fascia sign measuring 0.4m high, 2.1m wide, at a height above ground of 2.8m (ii) two non-illuminated projecting signs measuring 0.45m high, 0.64m wide, at heights above ground of 2.74m and 2.88m respectively (iii) one internally illuminated internal hanging star sign measuring 1.17m high, 1.24m wide at a height above ground of 1.3m.	21.05.2015

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<b>Committee:</b>	<b>Date:</b>	
Planning and Transportation	23 June 2015	
<b>Subject:</b> Valid planning applications received by Department of the Built Environment		
<b>Public</b>		

1. Pursuant to the instructions of your Committee, I attach for your information a list detailing development applications received by the Department of the Built Environment since my report to the last meeting.
2. Any questions of detail arising from these reports can be sent to [plans@cityoflondon.gov.uk](mailto:plans@cityoflondon.gov.uk).

## DETAILS OF VALID APPLICATIONS

Application Number & Ward	Address	Proposal	Date of Validation
15/00298/FULL Aldersgate	97 Aldersgate Street, London, EC1A 4JR	Alterations to acoustic louvres.	08/05/2015
15/00523/FULL Bishopsgate	222 Bishopsgate, London, EC2M 4QD	Application under section 73 of the Town and Country Planning Act 1990 to vary condition 3 of planning permission dated 18/11/2015 (14/00530/FULL) for a revised position of the roller shutter headbox.	22/05/2015
15/00397/FULL Bread Street	1 Cheapside, London, EC2V 6AA	Installation of new shop front.	11/05/2015
15/00342/FULL Bridge And Bridge Without	9-10 Philpot Lane, London, EC3M 8AA	Application for a Minor Material Amendment under Section 73 of the Town & Country Planning Act 1990 to vary Condition 12 of a planning permission (application no. 14/00189/FULL) dated 12th June 2014 to incorporate alterations to include re-configuration to the restaurant entrance and bin store, coffee shop entrance, minor alterations to internal layout, flue extract terminating at roof level, relocation of roof plant and a new parapet wall set back 300mm from the existing wall.	07/05/2015

15/00411/FULL Bridge And Bridge Without	10 Eastcheap, London, EC3M 1AJ	Installation of an external terrace at 4th floor level.	14/05/2015
15/00545/FULL Bridge And Bridge Without	Fishmongers' Hall, London Bridge, London, EC4R 9EL	Construction of a new stone ramp and steps on Fishmongers' Wharf to improve accessibility of riverside walk with associated gates, handrails and guardrails. This involves the alterations of Fishmongers' Wharf which is a city walkway.	28/05/2015
15/00536/FULL Castle Baynard	1 Puddle Dock, London, EC4V 3DS	Refurbishment of existing office block including: (i) replacement of the existing window system (ii) replacement of the existing roof covering and provision of a green roof (iii) cleaning and repair of existing concrete panelling (iv) relocation of existing Queen Victoria Street office entrance and creation of a new office entrance to provide access from Queen Victoria Street and Puddle Dock.	29/05/2015
15/00589/FULL Castle Baynard	Blackfriars Bridge & Paul's Walk, London, EC4V	Lift and stairs to the north east of Blackfriars Bridge	02/06/2015
15/00503/FULL Coleman Street	118 London Wall, London, EC2Y 5JA	Addition of external ducting to the rear elevation of the property to allow for ventilation and mechanical extraction for a kitchen area at basement level.	18/05/2015
15/00505/FULL Coleman Street	Chartered Accountants Hall, 1 Moorgate Place, London, EC2R 6EA	Installation of new roof light within existing roof to 1 Moorgate Place.	02/06/2015
15/00510/FULL Coleman Street	Chartered Accountants Hall, 1 Moorgate Place, London, EC2R 6EA	Installation of ventilation louvres to north facade	02/06/2015
15/00496/FULL Cordwainer	1 Poultry, London, EC2R 8EJ	Refurbishment and alterations to including change of use from a shop (A1) to create a new office reception (B1) and flexible shop/office (A1/B1) use at ground floor level.	18/06/2015
15/00492/FULL Cripplegate	01 Andrewes House, Barbican, London, EC2Y 8AX	Change of use of the contractors office to 1no. self-contained residential dwelling (Use Class C3).	15/05/2015



15/00239/FULL Farringdon Within	68 Long Lane, London, EC1A 9EJ	(i) Change of use from Class B1 (Office) to Class C3 (Residential) (ii) extension and external alterations at third floor level (iii) single storey extension at roof level and formation of a roof terrace.	18/05/2015
15/00509/FULMAJ Farringdon Within	20 Farringdon Street, London, EC4A 4AB	Redevelopment providing a 12 storey building comprising office accommodation (Use Class B1) at basement to 11th floor (10,433sq.m) and public house (Use Class A4) at part basement and part ground floor (397sq.m), incorporating ancillary car parking, cycle parking and refuse storage (Total Floorspace 11,880sq.m GIA).	19/05/2015
15/00490/FULL Farringdon Within	2 Cloth Court & 55 Long Lane, London, EC1A 7LS	Change of use of the first, second and third floors (accessed from Cloth Court) from office use (Class B1) to residential (Class C3) in the form of two maisonettes; (ii) roof alterations including the addition of a mansard roof extension with two air conditioning units; (iii) Change of use of the basement from offices (Class B1) to flexible retail use (Classes A1, A2, A3); (iv) Change of use of the ground floor from shop use (Class A1) to flexible retail use (Classes A1, A2, A3).	22/05/2015
15/00486/FULL Farringdon Without	St Bartholomew's Hospital, West Smithfield, London, EC1A 7BE	Installation of bi-folding gates between the FM Building and KGV Block (off Little Britain).	14/05/2015
15/00456/FULLR3 Lime Street	St Helen's Bishopsgate,, Great St Helen's, Undershaft, London, EC3A 6AT	Temporary installation of a sculpture - Charity by Damian Hirst- for a temporary period of up to 1 year to be taken down on or before 05 June 2016.	01/05/2015
15/00565/FULLR3 Lime Street	Undershaft, Land Adjoining , 1 Great St Helen's, London, EC3	Temporary installation of a sculpture - Breakout II by Bruce Beasley - for a temporary period of up to 1 year to be removed on or before 05 June 2016.	28/05/2015
15/00408/FULL Portsoken	Outside Guinness Court, Mansell Street, London, E1 8AB	New communal bin store and the formation of a new access path.	21/05/2015

15/00476/FULL Tower	1 Aldgate, London, EC3N 1RE	Change of use from Class B1 (Office) to Class A1 (Retail) or A2 (Financial & Professional Services) at ground floor level, the installation of new entrance, ATM and ventilation louvres to replace three glazed windows.	13/05/2015
15/00488/FULL Tower	74 Aldgate High Street, London, EC3N 1BD	Installation of ATM cashpoint within Aldgate High Street elevation.	21/05/2015
15/00362/FULL Vintry	30 Cannon Street, London, EC4M 6XH	Alterations to the Bread Street entrance including new hard and soft landscaping and the re-cladding of a lift entrance.	13/05/2015

<b>Committee:</b>	<b>Date:</b>
Planning and Transportation	23 June 2015
<b>Subject:</b> Walsingham House 35 Seething Lane London EC3N 4AH External alterations including the replacement of the 7th floor, the addition of two storeys office (B1) use with a plant room at roof level; infilling of the lightwell and the sloping roof fronting onto Crutched Friars; replacement windows; new entrances; flexible office (B1) /retail (A1) use of part of the ground floor; ancillary roof plant and cycle spaces. 8,517sq.m (GEA).	<b>Public</b>
<b>Ward:</b> Tower	<b>For Decision</b>
<b>Registered No:</b> 14/01226/FULMAJ	<b>Registered on:</b> 22 December 2014
<b>Conservation Area:</b> Trinity Square	<b>Listed Building:</b> No

## Summary

Walsingham House is located at the corner of Crutched Friars, Seething Lane and Pepys Street. The existing building was constructed in 1929. It consists of basement, ground and eight upper storeys (44.8m AOD).

The proposal is for external alterations including the replacement of the 7th floor, the addition of two storeys for office (B1) use with a plant room at roof level; infilling of the lightwell and the sloping roof fronting onto Crutched Friars; flexible office (B1) /retail (A1) use of part of the ground floor; ancillary roof plant and cycle spaces.

The proposal would result in a building comprising of basement, ground, nine upper storeys and a roof plant storey. The extended building would be (53.18m AOD).

The proposal would increase the overall floorspace by 2,038sq.m to 8,517sq.m (GEA). This could be all office (B1) floorspace or up to 696sq.m (GEA) could be retail (A1) use within two units at ground floor and basement level.

The scheme provides an employment led, mixed use development which supports the economic policies of the adopted Local Plan and the London Plan and provides an increase in high quality floorspace in this part of the City.

One letter of support and one letter of objection have been received from 41 Crutched Friars. The objector is concerned about loss of daylight and sunlight.

The proposal sets back the upper floors from the main street frontage to

minimise the impacts upon its neighbours, including the residential units at Crutched Friars.

This development would not detract from the City's conservation areas, listed buildings and non-designated heritage assets or be detrimental to the setting of the Tower of London World Heritage Site.

The proposal is substantially in compliance with the provisions of the Development Plan.

It is concluded that the proposal is acceptable subject to conditions and to a Section 106 agreement being entered into to cover the matters set out in the report.

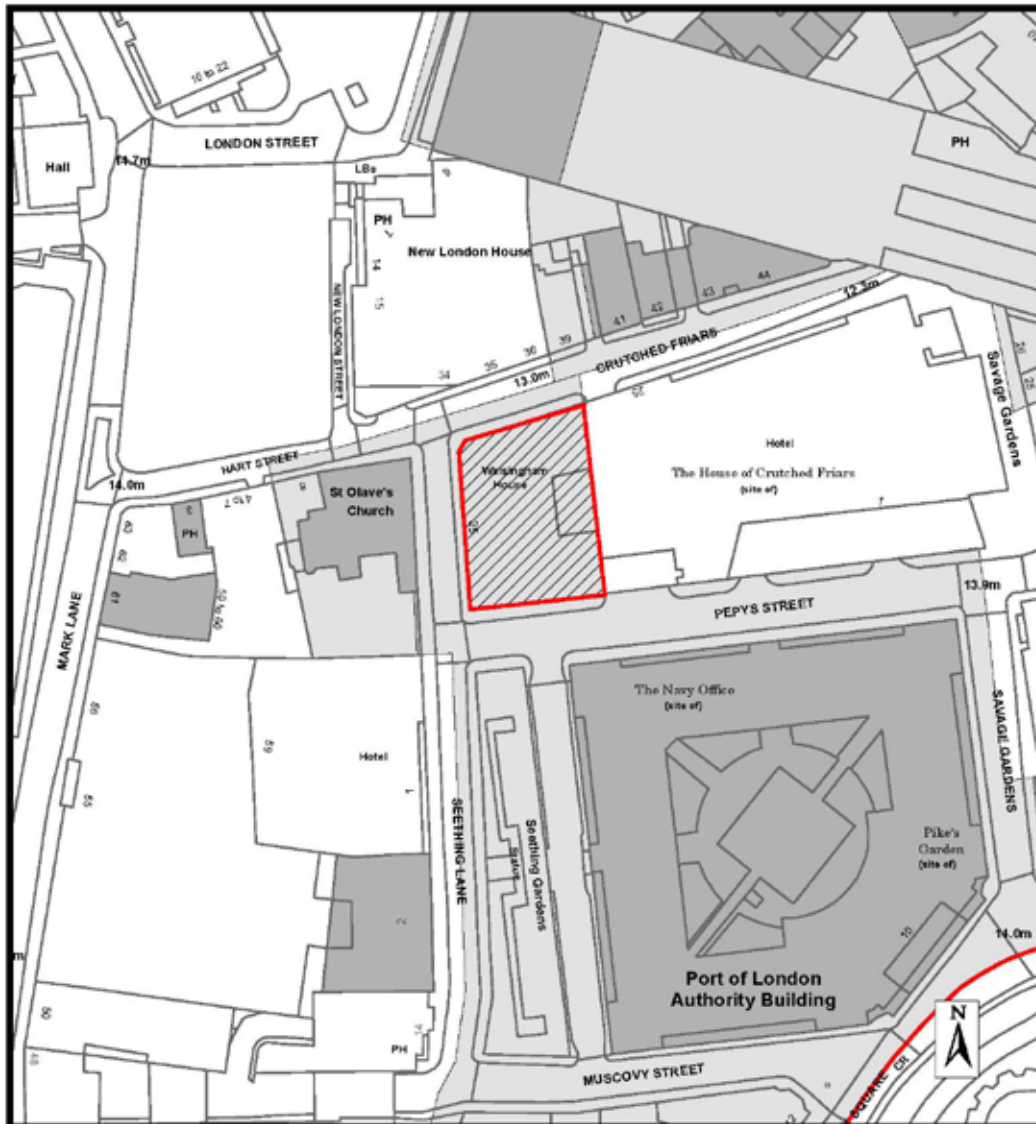
### **Recommendation**

(1) That you authorise the Chief Planning Officer to determine the above application for the above proposal in accordance with the details set out in the attached schedule subject to:

(a) planning obligations and other agreements being entered into under Section 106 of the Town & Country Planning Act 1990 and Section 278 of the Highway Act 1980 in respect of those matters set out in the report, the decision notice not to be issued until the Section 106 obligations have been executed;

(2) That your Officers be instructed to negotiate and execute obligations in respect of those matters set out in "Planning Obligations" under Section 106 and any necessary agreements under Section 278 of the Highway Act 1980.


# Site Location Plan



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ADDRESS:  
Walsingham House, 35 Seething Lane

CASE No.  
14/01226/FULMAJ

-  **SITE LOCATION**
-  **LISTED BUILDINGS**
-  **CONSERVATION AREA BOUNDARY**
-  **CITY OF LONDON BOUNDARY**



DEPARTMENT OF THE BUILT ENVIRONMENT



Existing view of Walsingham House from Seething Lane looking north

Case No. 14/01226/FULMAJ

## **Main Report**

### **Site**

1. Walsingham House is located at the corner of Crutched Friars, Seething Lane and Pepys Street.
2. The existing building was constructed in 1929. It has a basement, ground and eight upper storeys (44.8m AOD).
3. It comprises of 6,479sq.m (GEA) of office (B1) floorspace.
4. The building is not listed but lies within the Trinity Square Conservation Area.
5. To the west of the site on the corner of Hart Street and Seething Lane is St. Olave's Church which is listed Grade I. To the south of the site is 10 Trinity Square which is listed Grade II\*. To the north of the site is 41 Crutched Friars which is listed Grade II, 42 Crutched Friars which is listed Grade II\* and 43-44 Crutched Friars which is listed Grade II. The site is adjacent to the Double Tree Hilton Hotel.

### **Proposal**

6. Planning permission is sought for external alterations including the replacement of the 7th floor, the addition of two office (B1) storeys with a plant room at roof level; infilling of the lightwell and the sloping roof fronting onto Crutched Friars; replacement windows; new entrances; flexible office (B1) /retail (A1) use of part of the ground floor; ancillary roof plant and cycle spaces.
7. The proposal would result in a building with a basement, ground, nine upper storeys and a roof plant storey. The extended building would be (53.18m AOD).
8. The proposal would increase the overall floorspace by 2,038sq.m to 8,517sq.m (GEA). This could be all office (B1) floorspace or up to 696sq.m (GEA) could be retail (A1) use within two units at ground floor and basement level.
9. The main office entrance would remain on Seething Lane. The two retail units could have entrances on Pepys Street and Crutched Friars.

### **Consultations**

10. The views of other City of London departments have been taken into account in the preparation of this redevelopment scheme and some detailed matters remain to be dealt with under conditions and the Section 106 agreement.
11. Historic England does not wish to comment and says that the application should be determined in accordance with national and local policy guidance and on the basis of the City's specialist conservation advice.

12. One letter of objection has been received which states, *"I object to this application on the grounds that it reduces the levels of daylight and sunlight received by my 2 properties at Flat 2 and Flat 3 41 Crutched Friars, London EC3N 2AE to levels that are both unacceptable to me and non-compliant with BRE Guidelines. This application also breaches the terms of a permissive deed executed in 1929 as the roof is above 120 feet ordinance datum"*. The daylight and sunlight issues are considered later in this report. The point about the permissive deed is not a material planning consideration.
13. One letter of support has been received from a resident at Flat 1 41 Crutched Friars. They state, *'This is an example of how good architecture can enhance the appearance of an already distinguished building. The scale is appropriately grand for the site and will enable the owners to offer further and much better office accommodation for the City of London which is of course greatly needed'*.

### **Policy Context**

14. The development plan consists of the London Plan and the City of London Local Plan. The London Plan and Local Plan policies that are most relevant to the consideration of this case are set out in Appendix A to this report.
15. There is relevant City of London and GLA supplementary planning guidance in respect of Planning Obligations, Sustainable Design and Construction.
16. Government Guidance is contained in the National Planning Policy Framework (NPPF). Chapter 12 of the NPPF sets out key policy considerations for applications relating to designated and non-designated heritage assets. Other relevant guidance is provided by English Heritage including the documents Conservation Principles, and The Setting of Heritage assets, Building in Context (EH/CABE) and the PPS5 Practice Guide in respect of the setting of heritage assets.



## **Considerations**

17. The Corporation, in determining the planning application has the following main statutory duties to perform:-
- to have regard to the provisions of the development plan, so far as material to the application and to any other material considerations. (Section 70 Town & Country Planning Act 1990);
  - to determine the application in accordance with the development plan unless other material considerations indicate otherwise. (Section 38(6) of the Planning and Compulsory Purchase Act 2004).
  - In considering whether to grant planning permission for development which affects a listed building or its setting, to have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses. (S66 (1) Planning (Listed Buildings and Conservation Areas) Act 1990). In this case the duty is to the desirability of preserving the setting of listed buildings;
  - For development within or adjoining a conservation area, special attention shall be paid to the desirability of preserving or enhancing the character or appearance of that area and its setting (S72 (1) Planning (Listed Buildings and Conservation Areas) Act 1990);
  - to have regard to the purpose of conserving biodiversity as required by Section 40 of the Natural Environment and Rural Communities Act 2006;

### **The principal issues in considering this application are:**

- The extent to which the proposals comply with Government policy advice (NPPF);
- The extent to which the proposals comply with the relevant policies of the Development Plan;
- The impact of the proposal on designated and non-designated heritage assets;
- The impact on the nearby buildings and spaces, including daylight/sunlight and amenity.

### **Economic Development Issues**

18. London's status as a world city is founded to a substantial degree on its concentration of international service activities and, most noticeably, by the clustering of financial and business services in the City of London.
19. The importance that is attached to the maintenance and enhancement of the City's role as one of the world's leading financial and business centres is reflected in the policies of the London Plan and Local Plan.
20. The building would provide high quality office accommodation to meet the demands of the City's occupiers. The proposed building would provide high quality office accommodation and a maximum of 24% increase in office space over that which currently exists on the site.

## Retail

21. If the retail (A1) use is implemented there could be a total of up to 696sq.m (GEA) within two units at ground floor and basement level.
22. The proposed retail units are not on a retail link but, if implemented, would form a local retail frontage. This is supported by Local Plan Policy DM20.3.

## Design

### Existing Building

23. The building's principal facade fronts Seething Lane but the building has facades of similar quality to Crutched Friars to the north and Pepys Street to the south. At the rear the building has party wall junctions with the adjoining building and a central lightwell area. Walsingham House is located within the Trinity Square conservation area and forms part of the setting of the Grade I listed church of St Olave Hart Street, the Grade II\* listed former Port of London Authority headquarters building, Grade II listed 41 Crutched Friars, Grade II\* 42 Crutched Friars and Grade II listed 43-44 Crutched Friars.
24. Walsingham House is constructed in Portland stone and has a free classical appearance with subtle art-deco elements. The lower floors of the building are well composed with a two story rusticated base supporting a giant order of plain pilasters that extend a further three floors higher, interspersed by vertical strips of metal Crittal windows.
25. The corners of the building have a more solid expression and rise higher than the cornice line that extends across the central section of each facade. The verticals bands of glazing running up the face of the southern corner elements terminate at their upper levels in a round headed arch. The fenestration is characterised by a clear hierarchy of windows with sub-divided glazing in painted metal window frames; small panes on the upper floors and larger sub-divisions at ground floor level. Each floor is marked by painted metal spandrel panels. The windows are recessed into stone faced returns with the exception of the ground floor windows which take the form of shallow chamfered bays which project out from the wind reveals to add to the impression of solidity at the rusticated base of the building. The impressive double height principal entrance is located at the centre of the Seething Lane frontage and is reached by three shallow steps.

26. The well-ordered formality of the base and middle parts of the Walsingham House building is not continued to the top of the building. The consistency of the lower floors breaks down at fifth floor level and above. The form of the building above this height appears to have been shaped by rights to light factors with a sloping roofline on the Seething Lane frontage and a distinct change in height between the north and southern frontages. This results in an unsatisfactory and unbalanced appearance when seen from street level. The later additions of an eighth floor caretaker's flat and the unsightly plant rooms, which are clearly visible looking north along Seething Lane, also detract from the appearance of the lower floors.

#### Principal External Changes

27. The principal external changes that would be made to the building are:
- Replacement of the 7th floor, the addition of two new storeys and a plant room at roof level.
  - Alteration to the ground floor frontages.
  - Replacement of the glazing.
  - Infilling of the rear lightwell.

#### Replacement 7<sup>th</sup> Floor, New 8<sup>th</sup> and 9<sup>th</sup> Floors and Plant Rooms.

28. The upper floors of the building would be remodelled and extended. The architectural appearance of the new extensions would draw on the appearance of the lower parts of the building and would be in Portland Stone. The alterations would address the unbalanced appearance of the principal Seething Lane facade and would increase the height of the Crutched Friars facade to a height consistent with the other two street facades. The north facing sloping roof and the northeast-southwest height discrepancy of upper floors across the site would be removed. The existing parapet lines to the upper floors would be regularised and the new upper floors would be formed in repeating set-backs to mirror the precedents on the existing levels 6 and 7. The new top floor would be embellished with decorative metal screening above the windows to further enhance the appearance of the upper levels of the building.
29. A new plant room would be provided above the 9<sup>th</sup> floor but this would be sufficiently set back and angled to ensure that it would not be visible in views from street level. Glass balustrades would be provided to new terrace areas around the perimeter of the new floors.

### Alterations to the Ground Floor Frontages

30. The main entrance to the building is currently accessed via three shallow steps. The floor level to the office reception area would be lowered and an internal platform lift provided to allow a level entry into the building from the street. The timber entrance screen would be retained but the current double doors would be replaced by a set of automatic sliding timber doors that would match the appearance and proportions of the existing doors. The pattern of glazing above the door would be simplified.
31. The proposals envisage the use of part of the ground floor for retail uses which would require the formation of two new entrances. Two existing window bays would be sympathetically adapted to create new accessible entrances whilst maintaining the coherent appearance of the building's ground floor facade.

### Replacement Glazing.

32. The 1930's single glazed metal framed Crittal style windows would be replaced with powder coated steel framed, thermally broken, double glazed replicas with clear glass. At ground level the existing metal chamfered projecting bays would be replaced in replica and the varying pattern of sub-division to the ground floor glazing would be regularised. The existing rosette and lozenge decorations on these bays would either be reused or used to make replica moulds.

### Infilling of the Rear Lightwell.

33. The existing lightwell at the rear of the building would be infilled by a new building core with toilet accommodation to allow the creation of modern office floorspace within the building.

### Setting of Listed Buildings, Impact on the Conservation Area & Local Views.

34. Listed buildings are located sufficiently close to the site for their settings to be potentially affected by the proposed development. These buildings are the Grade I listed church of St Olave Hart Street, Grade II\* listed former headquarters building of the Port of London Authority (PLA), Grade II listed 41 Crutched Friars, Grade II\* 42 Crutched Friars and Grade II listed 43-44 Crutched Friars . The building is located within the Trinity Square conservation area.
35. Walsingham House is located directly across Seething Lane from the Church of St Olave. Careful consideration has been given by the architects to the form of the new floors that would be added to Walsingham House to ensure that the church would not be visually overwhelmed. St Olave's Church is of a modest scale - the east front that backs onto Seething Lane comprises single storey aisles flanking a double height nave. The church and Walsingham House are not seen together in longer views along Seething Lane as the church is masked by the buildings to the south. In the more immediate locality, the manner in which the new upper floors would be set back from the cornice line means that these floors would not be apparent except in steep upward looking views.

36. In views looking east along Hart Street the building already has a significant presence as a backdrop to St Olave's. The proposals would have a positive impact on the setting of the church and the street scene in general. The proportions and appearance of Walsingham House would be improved, the highly visible and unsightly plant structure at the top of the building would be removed, and the flank wall to the adjacent Mint Hotel building would be obscured by the new upper level of the northern facade of Walsingham House.
37. In views along Crutched Friars, due to the narrowness of the street and the manner in which the existing upper levels are set back from the pavement line, the existing glimpsed view of the churchyard and gate of St Olave's would be essentially unchanged.
38. The impact of the Walsingham House alterations on the setting of the former PLA building would be beneficial. There would be a noticeable improvement in the general street scene along Seething Lane. The design of the proposed new top storeys would be visually complimentary both to the architecture of the PLA building in terms of materials, massing and details, and to the general view looking northwards along the street.
39. In the Conservation Area Character Summary and Management Plan Walsingham House is identified as making a positive contribution to the conservation area and that it has remained unaltered. It is considered that the current proposals retain the positive attributes of the existing building and successfully address its shortcomings in a manner that would enhance the conservation area.
40. The proposed alterations are welcomed on the basis that the top of the building would be given a more architecturally coherent form in an appropriate architectural style and massing, using Portland stone to match its lower levels. The alterations would improve the appearance of the building and would enhance the conservation area.

#### Daylight and Sunlight

41. A report has been submitted which reviews the potential impact on daylight and sunlight of the proposal on surrounding properties using the British Research Establishment (BRE) guidelines.
42. Policy DM10.7 is,
  - (1) To resist development which would reduce noticeably the daylight and sunlight available to nearby dwellings and open spaces to unacceptable levels, taking account of the Building Research Establishment's guidelines.

43. The Building Research Establishment (BRE) guidelines set out several methods of assessing changes in daylight and sunlight arising from new developments. The Local Plan states that the City Corporation will apply these methods, consistent with BRE advice that ideal daylight and sunlight conditions may not be practicable in densely developed city-centre locations. An assessment needs to be made as to whether the impacts are reasonable in all the circumstances of any particular project and its context.
44. Residential properties are located to the north of the site at 39 and 41 Crutched Friars; the rest are commercial.
45. To assess the surrounding properties, the BRE guidelines provide two main methods for assessing daylight: Vertical Sky Component (VSC) which measures daylight received on the external plane of windows; Daylight Distribution/No Sky Line (NSL) which seeks to ensure that 80% of a room would have a view of the sky.
46. The VSC value is a measurement of the amount of 'visible sky' that can be seen from over and around an obstruction. It represents the amount of direct light from the sky received by a particular window. The maximum VSC value that can be received for a totally unobstructed vertical window is 40% VSC.
47. The BRE Guidance states, "If the VSC is greater than 27% then enough skylight should still be reaching the window of the existing building. Any reduction below this level should be kept to a minimum. If the VSC, with the new development in place, is both less than 27% and less than 0.8 times of its former value, occupants of the existing building will notice the reduction in the amount of skylight. The area lit by the window is likely to appear more gloomy, and electric lighting will be needed more of the time."
48. Sunlight is measured in terms of the percentage of annual probable sunlight hours (APSH) at the centre point of the window. The Guidance says that sunlight to an existing dwelling may be adversely affected if the window receives less than 25% of APSH or less than 5% of APSH during the winter, receives less than 0.8 times its former value during either period and has a reduction over the whole year of greater than 4% APSH.

### 39 Crutched Friars

#### Daylight

49. There are 20 windows serving seven residential rooms on the first to fifth floors of 39 Crutched Friars.
50. The existing VSC values for the windows at 39 Crutched Friars are all below 27%, ranging from 7.56% to 23.89%.

51. VSC assessment for 39 Crutched Friars show that five of the twenty windows tested would retain at least 0.8 times their existing values and the reductions would not be noticeable therefore. These five windows are located at the fourth and fifth floor levels. The reduction in daylight to the other 15 windows would retain between 0.70 and 0.79 of their former values.
52. The Daylight Distribution (NSL) test results show that three of the seven rooms would retain at least 0.8 times their former value and the reductions would not be noticeable therefore. The remaining four rooms would retain 0.43 to 0.73 of the former value.
53. A number of the results above are only marginally outside the guidelines. On the first and second floor, where the VSC losses would be in the noticeable range the NSL change would not be noticeable. In two rooms both the VSC and NSL changes would be noticeable. Overall, it is considered that this change would be a minor adverse impact.

#### Sunlight

54. All of the windows to rooms at 39 Crutched Friars currently exceed the minimum recommended level of 25% APSH but windows at first and second floors have less than the 5% recommended for winter months.
55. The APSH results show that six of the seven rooms would continue to have access to annual sunlight levels in excess of the BRE guidelines target of 25% APSH. The remaining room, at first floor level, and one window at second floor level would have less (20% - 23%) and the reductions would be 0.74 to 0.79 of their former value, and therefore be a noticeable change. Winter sunlight changes would be more significant with noticeable reductions to below the recommended 5% at 11 windows.

#### 41 Crutched Friars

#### Daylight

56. There are 12 windows serving three residential rooms on the first to third floors of 41 Crutched Friars
57. The results of the VSC assessment show that the reduction in daylight at all the windows would retain at least 0.8 times their existing values. The changes would not be noticeable therefore.
58. The daylight distribution results show that retained daylight areas are 0.63 at first floor level, 0.55 at second floor level and 0.59 at third floor level, compared to the 0.8 target. It should be noted that these rooms are greater than 5m deep, for which the BRE guide states that a "greater movement of the no skyline maybe unavoidable".
59. The results in respect of 41 Crutched Friars demonstrate that, whilst the NSL changes would be in the noticeable the VSC losses would not be. It is considered that this change would be a minor adverse impact.

### Sunlight

60. The windows at first and second floors of 41 Crutched Friars do not currently meet the minimum recommended level of 25% APSH and 5% for winter months. Those at third floor mainly exceed the minimum recommended level of 25% APSH, although one window has less than the 5% recommended for winter months.
61. The APSH results show annual sunlight levels would be reduced at all windows but would retain at least 0.8 times their existing sunlight values in all but one window on the first floor.
62. Winter APSH is currently well below the 5% guidance in all but three windows on the third floor. The proposals would reduce the winter APSH such that all would be below 5%. The changes would be by a noticeable amount in all but two windows on the first floor.

### Daylight and Sunlight Summary

63. The submitted report demonstrates that daylight to windows serving residential premises at 39 and 41 Crutched Friars and daylight distribution within these flats would be reduced but that the reductions would, in many cases, be at a level that would not be noticeable. Similarly, the annual and winter sunlight levels would be reduced, in some cases by amounts that would be a noticeable change.
64. These properties already have lower daylight and sunlight levels than are recommended by the BRE because of the built up nature of the surrounding vicinity, in particular the Hilton Doubletree hotel. As a result, these properties rely more heavily on the light being received from over the development site, and are therefore sensitive to any increase in massing.
65. The proposed scheme for Walsingham House sets back the upper floors from the main street frontage to reduce the impact upon its neighbours, including the residential properties at Crutched Friars.
66. Whilst there would be reductions in daylight and sunlight that would be of a noticeable level according to the BRE Guidance it is considered that these changes would be of a minor adverse impact and would not be so great as to be unacceptable.

### Sustainability & Energy

67. As part of the supporting documentation, the applicants have submitted an Energy Strategy Report, a BREEAM pre-assessment and Sustainability Statement.



68. The refurbishment and extension of the building provides the opportunity to improve its energy performance over the Building Regulations compliant baseline. Passive design measures, such as the installation of high-performance glazing, a new energy efficient building envelope for the extension, energy efficient lighting and controls and specification of energy efficient plant would be incorporated. The connection of the development into a district heating network would currently not be possible but the opportunity for a future connection would be provided. The installation of air source heat pumps is the only proposed renewable technology.
69. These measures are estimated to achieve a 32.75% improvement on carbon emissions savings over the performance of the existing building.
70. The new extension would not exceed the threshold for extensions to be considered as a new built element under the Building Regulations, and therefore would not be required to be assessed as new development.
71. The retrofitting and improvement of the development as a whole in relation to carbon emissions savings is considered to deliver substantial benefits towards sustainability and climate change adaptation and therefore is considered to be acceptable. This is confirmed by the anticipated BREEAM pre-assessment rating of "excellent".

#### Servicing & Parking

72. Servicing would be via an entrance on Pepys Street and from Seething Lane as in the existing situation. The existing number of deliveries per day is 12-13. The submitted servicing survey estimates that the number of deliveries would increase to 17-18 per day due to the additional office floorspace. The retail use, if implemented, would generate a greater demand for deliveries than the office use. The number of extra deliveries for the retail use would depend on the end occupier. The implementation of a Delivery and Servicing Plan is considered to mitigate any potential impact of the increase in the number of daily deliveries for both the office and retail use.
73. The existing development provides no car, motorcycle or cycle parking.
74. No on-site disabled parking would be provided. On-street disabled parking facilities are available on Seething Lane approximately 50m south of the office entrance.
75. The site is identified as being located in an area with a Public Transport Accessibility Level (PTAL) rating of 6b. This is the highest level of accessibility and rated as "Excellent".
76. There would be 68 cycle parking spaces provided at basement level together with shower and changing facilities. This would comply with the cycle parking standards in the Local Plan of 1 space per 125sq.m of floorspace (GEA).

### Access

77. Level access for people with disabilities would be provided into the proposed office and to the retail units if these are implemented.
78. Level access would be provided to the terraces at 8th and 9<sup>th</sup> floor levels. The intention is to provide level access to the terrace at 7th floor level but this is subject to the existing building structure allowing.

### Archaeology

79. The site is in an area of high archaeological potential where remains from all periods may be expected to survive. Roman, medieval and post medieval structural remains have been recorded in the area and the site is close to the precinct boundary of the medieval House of the Crutched Friars.
80. The proposals infill an existing lightwell, underpinning and strengthening works to foundations and new piled foundations which would have an impact on surviving archaeological remains.
81. The design of underpinning, strengthening works and new foundations would be informed by future soil investigation works. Conditions are recommended to cover archaeological evaluation, a programme of archaeological work and foundation design.

### **Planning Obligations and Community Infrastructure Levy**

82. Under Section 106 of the Town & Country Planning Act 1990 an agreement or planning obligation can be made between parties, usually the developer and the local authority, or a unilateral undertaking can be submitted by a prospective developer:
  - restricting the development or use of land in any specified way;
  - requiring specified operations or activities to be carried out in, on or under or over the land;
  - requiring the land to be used in any specified way; or
  - requiring a sum or sums to be paid to the authority on a specified date or dates or periodically.
83. Planning obligation arrangements were modified by the Community Infrastructure Levy Regulations 2010 as amended ('the CIL Regulations'). The Regulations introduce statutory restrictions on the use of planning obligations to clarify their proper purpose, and make provision for planning obligations to work alongside any Community Infrastructure Levy ('CIL') arrangements which local planning authorities may elect to adopt.

84. Regulation 122 states that it is unlawful for a planning obligation to constitute a reason to grant planning permission when determining a planning application if the obligation does not meet all the following tests:
- necessary to make the development acceptable in planning terms;
  - directly related to the development; and
  - fairly and reasonably related in scale and kind to the development
85. Regulation 123 states that a planning obligation may not constitute a reason to grant planning permission to the extent that it provides funding for infrastructure included in the regulation “Regulation 123” list as the type of infrastructure on what CIL will be spent on.
86. The National Planning Policy Framework (March 2012) stated that planning obligations should only be used where it is not possible to address unacceptable impacts through a planning condition. The policy repeated the tests set out above and states that where planning obligations are being sought or revised, local planning authorities should take account of changes in market conditions over time and, wherever appropriate, be sufficiently flexible to prevent planned development being stalled. (NPPF paragraphs 203-206).

#### **Mayoral Community Infrastructure Levy (CIL)**

87. London Plan Policy 8.3 requires the Mayoral CIL to be paid by developers to help fund strategically important infrastructure, initially focussing on Crossrail until 2019. The Mayor has set a charge of £50 per sq.m and this applies to all development over 100sq.m (GIA) except social housing, education related development, health related development and development for charities for charitable purposes.

#### **Mayoral Planning Obligations**

88. Since April 2010 the Mayor of London has sought contributions towards the cost of funding Crossrail through the negotiation of planning obligations in accordance with London Plan Policy 6.5. Mayoral planning obligations are payable by developers according to an indicative level of charges for specific uses set out in the Mayoral SPG (April 2013): offices (£140 per sq.m net gain in GIA floorspace), retail (£90) and hotels (£61) provided there is a net gain of 500sq.m.
89. Developments liable for both Mayoral CIL and Mayoral planning obligations payments for Crossrail will not be double charged. The Mayor will treat the CIL liability as a credit towards Mayoral planning obligation contribution. Therefore the Mayoral planning obligation liability can be reduced by the Mayoral CIL.

90. At the time of preparing this report the Mayoral CIL has been calculated to be £101,900. The full Mayoral planning obligation has been calculated to be £250,520. This would be reduced to £148,620 after deduction of the Mayoral CIL. It should be noted that these figures may be subject to change should there be a variation in the CIL liability at the point of payment and should therefore only be taken as indicative at this point.
91. Under the CIL regulations the City Corporation is able to retain 4% of the Mayoral CIL income as an administration fee; the remainder will be forwarded to the Mayor of London. The whole of the Mayoral planning obligation received will be forwarded to the Mayor. However, the developer will also be liable to pay an additional £3,500 Mayoral administration and monitoring charge to the City Corporation. The total contributions due in accordance with the Mayoral CIL and Mayoral planning obligation policies are summarised below:

<b>Liability in accordance with the Mayor of London's policies</b>	<b>Contribution £</b>	<b>Forwarded to the Mayor</b>	<b>Retained by City Corporation</b>
Mayoral Community Infrastructure Levy payable	101,900	97,824	4,076
Mayoral planning obligation net liability*	148,620	148,620	Nil
Mayoral planning obligation administration and monitoring charge	3,500	Nil	3,500
<b>Total liability in accordance with the Mayor of London's policies</b>	<b>£ 254,020</b>	<b>£246,444</b>	<b>£7,576</b>

\*Net liability is on the basis of the CIL charge remaining as reported and could be subject to variation.

### **City CIL**

92. The City introduced its CIL on 1<sup>st</sup> July 2014 and will be chargeable in addition to the Mayoral CIL and Mayoral planning obligations. CIL will be charged at a rate of £75 per sq.m for Offices, £150 for Residential Riverside, £95 for Residential rest of the city and £75 for all other uses. At the time of preparing this report the City CIL has been calculated to be £152,850. It should be noted that these figures may be subject to change should there be a variation in the CIL liability at the point of payment and should therefore only be taken as indicative figures at this point.

93. Under the CIL regulations the City Corporation is able to retain 5% of the CIL income as an administration fee. The contributions collected will be used to fund the infrastructure required to meet the requirements of the City's Development Plan.

**City S106 Planning Obligations**

94. On 1 July 2014 the City's Supplementary Planning Document on Planning Obligations was adopted. City Planning Obligations would be payable by developers in accordance with the Planning Obligations SPD on new commercial developments where there is a net increase of 500sq.m or more of Gross Internal Area. The policy seeks contributions towards Affordable Housing (£20 per sq.m), Local Training, Skills and Job Brokerage (£3 per sq.m) and Carbon Offsetting (£60 per tonne of carbon offset).
95. The section 106 agreement would normally follow the agreement template available on the City of London website.
96. In this case the proposed net increase in floorspace would be 2038sq.m. On the basis of the figure indicated in the Supplementary Planning Document, the planning obligation figure would be £46,874. It is the City's practice that all financial contributions be index-linked with reference to the appropriate index from the date of adoption of the City's SPD to the date planning permission was granted.

97. The applicant has agreed a breakdown which accords with the Supplementary Planning Document as follows:

<b>Liability in accordance with the City of London's policies</b>	<b>Contribution £</b>	<b>Available for Allocation £</b>	<b>Retained for Administration and Monitoring £</b>
City Community Infrastructure Levy	<b>152,850</b>	<b>145,208</b>	<b>7,642</b>
City Planning Obligation Affordable Housing	<b>40,760</b>	<b>40,352</b>	<b>408</b>
City Planning Obligation Local, Training, Skills and Job Brokerage	<b>6,114</b>	<b>6,053</b>	<b>61</b>
City Planning Obligation Monitoring Charge (non-financial obligations)	<b>1,500</b>	<b>Nil</b>	<b>1250 tbc</b>
<b>Total liability in accordance with the City of London's policies</b>	<b>£201,224</b>	<b>£191,613</b>	<b>£9,611</b>

98. I have set out below the details that I am recommending concerning the planning obligations. All of the proposals are considered to be necessary to make the application acceptable in planning terms, directly related to the development and fairly and reasonably related in scale and kind to the development and meet the above tests contained in the CIL Regulations and in government policy. I would also request that I be given delegated authority to continue to negotiate and agree the terms of the proposed obligations as necessary.

#### Affordable Housing Contribution

99. The Affordable Housing contribution will be used for the purpose of off-site provision of affordable housing in suitable locations in or near to the City of London in accordance with the London Plan. The applicant will be required to pay this contribution on or before the implementation of the planning permission.

#### Local Training, Skills and Job Brokerage Contribution

100. The Local Training, Skills and Job Brokerage contribution will be applied to the provision of training and skills initiatives, including job brokerage, in the City or City fringes. The Developer will be required to pay this contribution on or before the implementation of planning permission.

#### Delivery and Servicing Management Plan

101. The developer would be required to submit for approval a Delivery and Servicing Management Plan prior to occupation. In the event of any breach of the Management Plan, the developer will be required to resubmit a revised document, and should the developer default on this requirement, the City will be given the ability to provide a replacement plan. The operation of the Delivery and Servicing Management Plan will be subject to an annual review.

#### Travel Plan

102. The developer would be required to submit both interim and full Travel Plans prior to occupation and six months after occupation respectively. The obligations in relation to this shall apply for the life of the building

#### Local Training, Skills and Job Brokerage Strategy (Construction)

103. The applicant will be required to submit for approval details of the Local Training, Skills and Job Brokerage Strategy in line with the aims of the City Corporation's Employment Charter for Construction. This Charter aims to maximise job opportunities in the City for residents of the City fringes and offer employment and training opportunities to local people wishing to begin a career in construction. The Strategy will be submitted in two stages: one to be submitted prior to the First Preparatory Operation Date in respect of the Preparatory Operations; the second to be submitted prior to Implementation in respect of the Main Contract Works Package.
104. The Economic Development Office is able to introduce the Developer or its Contractor and Sub-Contractors to local training providers and brokerage agencies to discuss their site-specific skills needs and to identify suitable local people to fill opportunities on site. The Developer is encouraged to liaise with the Economic Development Office at the earliest stage in the development process in order that the strategy can be submitted prior to commencement.

#### Local Procurement

105. The applicant will be required to submit for approval a Local Procurement Strategy prior to commencement of demolition. The Local Procurement Strategy shall include details of: initiatives to identify local procurement opportunities relating to the construction of the development; initiatives to reach a 10% target for local procurement, from small to medium sized enterprises in the City and City fringes; the timings and arrangements for the implementation of such initiatives; and suitable mechanisms for the monitoring of the effectiveness of

such initiatives e.g. a local procurement tracker can be used to capture this information.

106. The developer will be required at the 6 month stage, or half way through the project (whichever is earliest), to report to the City of London Corporation's Economic Development Office on their performance against the 10% local procurement target.
107. The Economic Development Officer is able to provide information and guidance to the Developer its Contractor and Sub-Contractors. The Developer is encouraged to liaise with the Economic Development Officer at the earliest stage in the development process in order that the strategy can be submitted prior to implementation

#### Carbon Offsetting

108. The London Plan sets a target for major developments to achieve an overall carbon dioxide emission reduction of 40% from 2013-2016, through the use of on-site renewable energy generation. The submitted energy statement demonstrates that the development is designed to achieve a 34% reduction in carbon emissions when compared with the London Plan target of 35% improvement over the 2013 building regulations.
109. A detailed post construction assessment will be required. If the assessment demonstrates that the target is not met on site the applicant will be required to meet the shortfall through cash in lieu contribution. The contribution will be secured through the section 106 agreement at an initial cost of £60 per tonne of carbon to be offset, calculated over a 30 year period. The financial contribution for carbon off-setting will be required on Completion prior to occupation of the development.

#### Utility Connections

110. The development will require connection to a range of utility infrastructure. Early engagement by the applicant about utilities infrastructure provision will allow for proper co-ordination and planning of all works required to install the utility infrastructure, particularly under public highway, so as to minimise disruption to highway users. A s106 covenant will therefore require the submission of draft and final programmes for ordering and completing service connections from utility providers in order that the City's comments can be taken into account, and will require that all connections are carried out in accordance with the programme. Details of the utility connection requirements of the Development including all proposed service connections, communal entry chambers, the proposed service provider and the anticipated volume of units required for the Development will also be required.



### Monitoring and Administrative Costs

- 111. A 10 year repayment period would be required where by any unallocated sums would be returned to the developer 10 years after practical completion of the development. Some funds may be set aside for future maintenance purposes.
- 112. The applicant will pay the City of London's legal costs incurred in the negotiation and execution of the legal agreement and the City Planning Officer's administration costs in respect of the same. 1% of the total contribution (secured under the City's SPG) and £250 per non-financial obligation will be allocated to the monitoring of the agreement.
- 113. Separate additional administration and monitoring fees will be applied in relation to the Crossrail Contribution.

### Conclusions

- 114. The proposal supports the City's strategic objective to promote the City as the leading international financial and business centre.
- 115. The scheme provides an employment led, mixed use development which supports the economic policies of the adopted Local Plan and the London Plan and provides an increase in high quality floorspace in this part of the City.
- 116. This development would not detract from the City's conservation areas, listed buildings and non-designated heritage assets or be detrimental to the setting of the Tower of London World Heritage Site.
- 117. Whilst there would be reductions in daylight and sunlight that would be of a noticeable level according to the BRE Guidance it is considered that these changes would be of a minor adverse impact and would not be so great as to be unacceptable.
- 118. The proposal is substantially in compliance with the provisions of the Development Plan.
- 119. It is concluded that the proposal is acceptable subject to conditions and to a Section 106 agreement being entered into to cover the matters set out in the report.
- 120. I recommend planning permission be granted as set out in the Recommendation and Schedule.

## **Background Papers**

### **Internal**

Email Environmental Health 6/2/2015

### **External**

Energy Strategy Report Issue 2.0 - 2/12/2014

BREEAM Offices 2008 Issue 03 - 5/12/2014

Transport Assessment - December 2014

Statement of Community Involvement - December 2014

Draft Delivery & Servicing Plan - December 2014

Archaeological Appraisal JLQ 0193 - 4/12/2014

Daylight & Sunlight Report MH/KW/ROL7011 - 26/2/2015

Framework Travel Plan - December 2014

Planning Statement - December 2014

Design and Access Statement - 15 December 2014

Existing Drawings - P00/098 Rev P1, P00/099 Rev P1, P00/100 Rev P1, P00/101 Rev P1, P00/102 Rev P1, P00/103 Rev P1, P00/104 Rev P1, P00/105 Rev P1, P00/106 Rev P1, P00/107 Rev P1, P00/108 Rev P1, P00/120 Rev P1, P00/121 Rev P1, P00/122 Rev P1, P00/123 Rev P1, P00/124 Rev P1, P00/125 Rev P1, P00/126 Rev P1, P00/130, P00/132 Rev P1, P00/133 Rev P1.

Email David Lonsdale - 14/1/2014

Letter English Heritage - 20/1/2015

Letter CAAC - 4/2/2015

Letter John Robertson Architects - 9/2/2015, 2/4/2015x2

Email JLL - 16/3/2015 & 2/6/2015

Heritage Statement - January 2015

Letter Anstey Horne - 3/3/2015

Sustainability Statement - 17/3/2015

Email Michael Will - 14/5/2015

## **Appendix A**

### **London Plan Policies**

The London Plan policies which are most relevant to this application are set out below:

Policy 2.10 Enhance and promote the unique international, national and London wide roles of the Central Activities Zone (CAZ) and as a strategically important, globally-oriented financial and business services centre.

Policy 2.11 Ensure that developments proposals to increase office floorspace within CAZ include a mix of uses including housing, unless such a

Policy 4.2 Support the management and mixed use development and redevelopment of office provision to improve London's competitiveness and to address the wider objectives of this Plan, including enhancing its varied attractions for businesses of different types and sizes.

Policy 4.3 Within the Central Activities Zone increases in office floorspace should provide for a mix of uses including housing, unless such a mix would demonstrably conflict with other policies in this plan.

Policy 5.2 Development proposals should make the fullest contribution to minimising carbon dioxide emissions.

Policy 5.3 Development proposals should demonstrate that sustainable design standards are integral to the proposal, including its construction and operation. Major development proposals should meet the minimum standards outlined in supplementary planning guidance.

Policy 6.3 Development proposals should ensure that impacts on transport capacity and the transport network are fully assessed.

Policy 6.5 Contributions will be sought from developments likely to add to, or create, congestion on London's rail network that Crossrail is intended to mitigate.

Policy 6.9 Developments should provide secure, integrated and accessible cycle parking facilities and provide on-site changing facilities and showers for cyclists, facilitate the Cycle Super Highways and facilitate the central London cycle hire scheme.

Policy 7.2 All new development in London to achieve the highest standards of accessible and inclusive design.

Policy 7.4 Development should have regard to the form, function, and structure of an area, place or street and the scale, mass and orientation of surrounding buildings. It should improve an area's visual or physical connection with natural features. In areas of poor or ill-defined character, development should build on the positive elements that can contribute to establishing an enhanced character for the future function of the area.

Policy 7.6 Buildings and structures should:

- a be of the highest architectural quality
- b be of a proportion, composition, scale and orientation that enhances, activates and appropriately defines the public realm
- c comprise details and materials that complement, not necessarily replicate, the local architectural character
- d not cause unacceptable harm to the amenity of surrounding land and buildings, particularly residential buildings, in relation to privacy, overshadowing, wind and microclimate. This is particularly important for tall buildings
- e incorporate best practice in resource management and climate change mitigation and adaptation
- f provide high quality indoor and outdoor spaces and integrate well with the surrounding streets and open spaces
- g be adaptable to different activities and land uses, particularly at ground level
- h meet the principles of inclusive design
- i optimise the potential of sites.

Policy 7.8 Development should identify, value, conserve, restore, re-use and incorporate heritage assets, conserve the significance of heritage assets and their settings and make provision for the protection of archaeological resources, landscapes and significant memorials.

Policy 7.13 Development proposals should contribute to the minimisation of potential physical risks, including those arising as a result of fire, flood and related hazards.

Policy 7.14 Implement Air Quality and Transport strategies to achieve reductions in pollutant emissions and minimise public exposure to pollution.

Relevant Local Plan Policies

### ***CS1 Provide additional offices***

To ensure the City of London provides additional office development of the highest quality to meet demand from long term employment growth and strengthen the beneficial cluster of activities found in and near the City that contribute to London's role as the world's leading international financial and business centre.

### ***DM1.5 Mixed uses in commercial areas***

To encourage a mix of commercial uses within office developments which contribute to the City's economy and character and provide support services for its businesses, workers and residents.

#### ***CS4 Seek planning contributions***

To manage the impact of development, seeking appropriate developer contributions.

#### ***CS10 Promote high quality environment***

To promote a high standard and sustainable design of buildings, streets and spaces, having regard to their surroundings and the character of the City and creating an inclusive and attractive environment.

#### ***DM10.1 New development***

To require all developments, including alterations and extensions to existing buildings, to be of a high standard of design and to avoid harm to the townscape and public realm, by ensuring that:

- a) the bulk and massing of schemes are appropriate in relation to their surroundings and have due regard to the general scale, height, building lines, character, historic interest and significance, urban grain and materials of the locality and relate well to the character of streets, squares, lanes, alleys and passageways;
- b) all development is of a high standard of design and architectural detail with elevations that have an appropriate depth and quality of modelling;
- c) appropriate, high quality and durable materials are used;
- d) the design and materials avoid unacceptable wind impacts at street level or intrusive solar glare impacts on the surrounding townscape and public realm;
- e) development has attractive and visually interesting street level elevations, providing active frontages wherever possible to maintain or enhance the vitality of the City's streets;
- f) the design of the roof is visually integrated into the overall design of the building when seen from both street level views and higher level viewpoints;
- g) plant and building services equipment are fully screened from view and integrated in to the design of the building. Installations that would adversely affect the character, appearance or amenities of the buildings or area will be resisted;
- h) servicing entrances are designed to minimise their effects on the appearance of the building and street scene and are fully integrated into the building's design;
- i) there is provision of appropriate hard and soft landscaping, including appropriate boundary treatments;
- j) the external illumination of buildings is carefully designed to ensure visual sensitivity, minimal energy use and light pollution, and the discreet integration of light fittings into the building design;
- k) there is provision of amenity space, where appropriate;
- l) there is the highest standard of accessible and inclusive design.

### ***DM10.7 Daylight and sunlight***

- 1) To resist development which would reduce noticeably the daylight and sunlight available to nearby dwellings and open spaces to unacceptable levels, taking account of the Building Research Establishment's guidelines.
- 2) The design of new developments should allow for the lighting needs of intended occupiers and provide acceptable levels of daylight and sunlight.

### ***DM10.8 Access and inclusive design***

To achieve an environment that meets the highest standards of accessibility and inclusive design in all developments (both new and refurbished), open spaces and streets, ensuring that the City of London is:

- a) inclusive and safe for all who wish to use it, regardless of disability, age, gender, ethnicity, faith or economic circumstance;
- b) convenient and welcoming with no disabling barriers, ensuring that everyone can experience independence without undue effort, separation or special treatment;
- c) responsive to the needs of all users who visit, work or live in the City, whilst recognising that one solution might not work for all.

### ***CS12 Conserve or enhance heritage assets***

To conserve or enhance the significance of the City's heritage assets and their settings, and provide an attractive environment for the City's communities and visitors.

### ***DM12.2 Development in conservation areas***

1. Development in conservation areas will only be permitted if it preserves and enhances the character or appearance of the conservation area.
2. The loss of heritage assets that make a positive contribution to the character or appearance of a conservation area will be resisted.
3. Where permission is granted for the demolition of a building in a conservation area, conditions will be imposed preventing demolition commencing prior to the approval of detailed plans of any replacement building, and ensuring that the developer has secured the implementation of the construction of the replacement building.

## ***CS15 Creation of sustainable development***

To enable City businesses and residents to make sustainable choices in their daily activities creating a more sustainable City, adapted to the changing climate.

### ***DM15.1 Sustainability requirements***

1. Sustainability Statements must be submitted with all planning applications in order to ensure that sustainability is integrated into designs for all development.
2. For major development (including new development and refurbishment) the Sustainability Statement should include as a minimum:
  - a) BREEAM or Code for Sustainable Homes pre-assessment;
  - b) an energy statement in line with London Plan requirements;
  - c) demonstration of climate change resilience measures.
3. BREEAM or Code for Sustainable Homes assessments should demonstrate sustainability in aspects which are of particular significance in the City's high density urban environment. Developers should aim to achieve the maximum possible credits to address the City's priorities.
4. Innovative sustainability solutions will be encouraged to ensure that the City's buildings remain at the forefront of sustainable building design. Details should be included in the Sustainability Statement.
5. Planning conditions will be used to ensure that Local Plan assessment targets are met.

### ***DM15.2 Energy and CO2 emissions***

1. Development design must take account of location, building orientation, internal layouts and landscaping to reduce likely energy consumption.

2. For all major development energy assessments must be submitted with the application demonstrating:
  - a) energy efficiency - showing the maximum improvement over current Building Regulations to achieve the required Fabric Energy Efficiency Standards;
  - b) carbon compliance levels required to meet national targets for zero carbon development using low and zero carbon technologies, where feasible;
  - c) where on-site carbon emission reduction is unviable, offsetting of residual CO<sub>2</sub> emissions through "allowable solutions" for the lifetime of the building to achieve national targets for zero-carbon homes and non-domestic buildings. Achievement of zero carbon buildings in advance of national target dates will be encouraged;
  - d) anticipated residual power loads and routes for supply.

### ***DM15.7 Noise and light pollution***

1. Developers will be required to consider the impact of their developments on the noise environment and where appropriate provide a noise assessment. The layout, orientation, design and use of buildings should ensure that operational noise does not adversely affect neighbours, particularly noise-sensitive land uses such as housing, hospitals, schools and quiet open spaces.
2. Any potential noise conflict between existing activities and new development should be minimised. Where the avoidance of noise conflicts is impractical, mitigation measures such as noise attenuation and restrictions on operating hours will be implemented through appropriate planning conditions.
3. Noise and vibration from deconstruction and construction activities must be minimised and mitigation measures put in place to limit noise disturbance in the vicinity of the development.
4. Developers will be required to demonstrate that there will be no increase in background noise levels associated with new plant and equipment.
5. Internal and external lighting should be designed to reduce energy consumption, avoid spillage of light beyond where it is needed and protect the amenity of light-sensitive uses such as housing, hospitals and areas of importance for nature conservation.

### ***CS16 Improving transport and travel***

To build on the City's strategic central London position and good transport infrastructure to further improve the sustainability and efficiency of travel in, to, from and through the City.



### ***DM16.3 Cycle parking***

1. On-site cycle parking must be provided in accordance with the local standards set out in Table 16.2 or, for other land uses, with the standards of the London Plan. Applicants will be encouraged to exceed the standards set out in Table 16.2.
2. On-street cycle parking in suitable locations will be encouraged to meet the needs of cyclists.

### ***CS17 Minimising and managing waste***

To support City businesses, residents and visitors in making sustainable choices regarding the minimisation, transport and management of their waste, capitalising on the City's riverside location for sustainable waste transfer and eliminating reliance on landfill for municipal solid waste (MSW).

#### ***DM17.1 Provision for waste***

1. Waste facilities must be integrated into the design of buildings, wherever feasible, and allow for the separate storage and collection of recyclable materials, including compostable material.
2. On-site waste management, through techniques such as recycle sorting or energy recovery, which minimises the need for waste transfer, should be incorporated wherever possible.

### ***CS20 Improve retail facilities***

To improve the quantity and quality of retailing and the retail environment, promoting the development of the five Principal Shopping Centres and the linkages between them.

#### ***DM20.3 Retail uses elsewhere***

To resist the loss of isolated and small groups of retail units outside the PSCs and Retail Links that form an active retail frontage, particularly A1 units near residential areas, unless it is demonstrated that they are no longer needed.

### ***DM12.4 Archaeology***

1. To require planning applications which involve excavation or ground works on sites of archaeological potential to be accompanied by an archaeological assessment and evaluation of the site, including the impact of the proposed development.

2. To preserve, protect, safeguard and enhance archaeological monuments, remains and their settings in development, and to seek a public display and interpretation, where appropriate.
3. To require proper investigation and recording of archaeological remains as an integral part of a development programme, and publication and archiving of results to advance understanding.

## SCHEDULE

APPLICATION: 14/01226/FULMAJ

**Walsingham House 35 Seething Lane London**

**External alterations including the replacement of the 7th floor, the addition of two storeys office (B1) use with a plant room at roof level; infilling of the lightwell and the sloping roof fronting onto Crutched Friars; replacement windows; new entrances; flexible office (B1) /retail (A1) use of part of the ground floor; ancillary roof plant and cycle spaces. 8,517sq.m (GEA).**

## CONDITIONS

- 1 The development hereby permitted shall be begun before the expiration of three years from the date of this permission.  
REASON: To ensure compliance with the terms of Section 91 of the Town and Country Planning Act 1990.
- 2 A scheme for protecting nearby residents and commercial occupiers from noise, dust and other environmental effects during construction shall be submitted to and approved in writing by the Local Planning Authority prior to any construction work taking place on the site. The scheme shall be based on the Department of Markets and Consumer Protection's Code of Practice for Deconstruction and Construction Sites and arrangements for liaison set out therein. A staged scheme of protective works may be submitted in respect of individual stages of the construction process but no works in any individual stage shall be commenced until the related scheme of protective works has been submitted to and approved in writing by the Local Planning Authority. The development shall not be carried out other than in accordance with the approved scheme.  
REASON: In the interests of public safety and to ensure a minimal effect on the amenities of neighbouring premises and the transport network in accordance with the following policies of the Local Plan: DM15.6, DM15.7, DM21.3.
- 3 Archaeological evaluation shall be carried out in order to compile archaeological records in accordance with a timetable and scheme of such archaeological work submitted to and approved in writing by the Local Planning Authority before any commencement of archaeological evaluation work.  
REASON: To ensure that an opportunity is provided for the archaeology of the site to be considered and recorded in accordance with the following policy of the Local Plan: DM12.4.

- 4 No works except demolition to basement slab level shall take place until the developer has secured the implementation of a programme of archaeological work to be carried out in accordance with a written scheme of investigation which has been submitted to and approved in writing by the Local Planning Authority. This shall include all on site work, including details of any temporary works which may have an impact on the archaeology of the site and all off site work such as the analysis, publication and archiving of the results. All works shall be carried out and completed as approved, unless otherwise agreed in writing by the Local Planning Authority.  
REASON: In order to allow an opportunity for investigations to be made in an area where remains of archaeological interest are understood to exist in accordance with the following policy of the Local Plan: DM12.4.
- 5 No works except demolition to basement slab level shall take place before details of the foundations and piling configuration, to include a detailed design and method statement, have been submitted to and approved in writing by the Local Planning Authority, such details to show the preservation of surviving archaeological remains which are to remain in situ.  
REASON: To ensure the preservation of archaeological remains following archaeological investigation in accordance with the following policy of the Local Plan: DM12.4.
- 6 (a) The level of noise emitted from any new plant shall be lower than the existing background level by at least 10 dBA. Noise levels shall be determined at one metre from the nearest window or facade of the nearest premises.  
The measurements and assessments shall be made in accordance with B.S. 4142. The background noise level shall be expressed as the lowest LA90 (10 minutes) during which plant is or may be in operation. Following installation but before the new plant comes into operation measurements of noise from the new plant must be taken and a report demonstrating that the plant as installed meets the design requirements shall be submitted to and approved in writing by the Local Planning Authority.  
(b) All constituent parts of the new plant shall be maintained and replaced in whole or in part as often is required to ensure compliance with the noise levels approved by the Local Planning Authority.  
REASON: To protect the amenities of neighbouring residential/commercial occupiers in accordance with the following policies of the Local Plan: DM15.7, DM21.3.

- 7 Before any works thereby affected are begun the following details shall be submitted to and approved in writing by the Local Planning Authority and all development pursuant to this permission shall be carried out in accordance with the approved details:
- (a) particulars and samples of the materials to be used on all external faces of the building including external ground and upper level surfaces;
  - (b) details of the proposed new facade(s) including typical details of the fenestration and entrances;
  - (c) typical details of stonework;
  - (d) details of ground floor elevations;
  - (e) details of the ground floor office and retail entrances;
  - (f) details of windows and external joinery;
  - (g) details of new dormer windows;
  - (h) details of soffits, hand rails and balustrades;
  - (i) details of junctions with adjoining premises;
  - (j) details of the integration of window cleaning equipment and the garaging thereof, plant, flues, fire escapes and other excrescences at roof level
- REASON: To ensure that the Local Planning Authority may be satisfied with the detail of the proposed development and to ensure a satisfactory external appearance in accordance with the following policies of the Local Plan: DM3.2, DM10.1, DM10.5, DM12.2.
- 8 Permanently installed pedal cycle racks shall be provided and maintained on the site throughout the life of the building sufficient to accommodate a minimum of 68 pedal cycles (at 1 to 125). The cycle parking provided on the site must remain ancillary to the use of the building and must be available at all times throughout the life of the building for the sole use of the occupiers thereof and their visitors without charge to the individual end users of the parking.
- REASON: To ensure provision is made for cycle parking and that the cycle parking remains ancillary to the use of the building and to assist in reducing demand for public cycle parking in accordance with the following policy of the Local Plan: DM16.3.
- 9 Changing facilities and showers shall be provided adjacent to the bicycle parking areas and maintained throughout the life of the building for the use of occupiers of the building in accordance with the approved plans.
- REASON: To make travel by bicycle more convenient in order to encourage greater use of bicycles by commuters in accordance with the following policy of the Local Plan: DM16.4.
- 10 At all times when not being used for cleaning or maintenance the window cleaning gantries, cradles and other similar equipment shall be garaged within the enclosure(s) shown on the approved drawings.
- REASON: To ensure a satisfactory external appearance in accordance with the following policy of the Local Plan: DM10.1.

- 11 The development shall be designed to allow for the retro-fit of heat exchanger rooms to connect into a district heating network if this becomes available during the lifetime of the development.  
REASON: To minimise carbon emissions by enabling the building to be connected to a district heating and cooling network if one becomes available during the life of the building in accordance with the following policies of the Local Plan: DM15.1, DM15.2, DM15.3, DM15.4.
- 12 A post construction BREEAM assessment demonstrating that a target rating of 'Excellent' has been achieved (or such other target rating as the local planning authority may agree provided that it is satisfied all reasonable endeavours have been used to achieve an 'Excellent' rating) shall be submitted as soon as practicable after practical completion.  
REASON: To demonstrate that carbon emissions have been minimised and that the development is sustainable in accordance with the following policy of the Local Plan: DM15.1.
- 13 The refuse collection and storage facilities shown on the drawings hereby approved shall be provided and maintained throughout the life of the building for the use of all the occupiers.  
REASON: To ensure the satisfactory servicing of the building in accordance with the following policy of the Local Plan: DM17.1.
- 14 No live or recorded music shall be played that it can be heard outside the premises or other premises in the building.  
REASON: To safeguard the amenity of the adjoining premises and the area in general in accordance with the following policies of the Local Plan: DM15.7, DM21.3.
- 15 The development shall not be carried out other than in accordance with the following approved drawings and particulars or as approved under conditions of this planning permission: P00/199 Rev P1, P00/200 Rev P1, P00/201 Rev P1, P00/202 Rev P1, P00/203 Rev P1, P00/204 Rev P1, P00/205 Rev P1, P00/206 Rev P1, P00/207 Rev P1, P00/208 Rev P1, P00/220 Rev P1, P00/221 Rev P1, P00/222 Rev P1, P00/223 Rev P1, P00/224 Rev P1, P00/225 Rev P1, P00/226 Rev P1.  
  
P01/099 Rev P3, P01/100 Rev P3, P01/101 Rev P1, P01/102 Rev P1, P01/103 Rev P1, P01/104 Rev P1, P01/105 Rev P1, P01/106 Rev P1, P01/107 Rev P1, P01/108 Rev P1, P01/109 Rev P1, P01/110 Rev P1, P01/111 Rev P1, P01/120 Rev P2, P01/121 Rev P2, P01/122 Rev P2, P01/123 Rev P1, P01/124 Rev P1, P01/125 Rev P1, P01/126 Rev P1, P01/130 Rev P1, P01/131 Rev P3, P01/132 Rev P1, P01/133 Rev P2, L06/001 Rev P2.  
REASON: To ensure that the development of this site is in compliance with details and particulars which have been approved by the Local Planning Authority.

## INFORMATIVES

- 1 In dealing with this application the City has implemented the requirements of the National Planning Policy Framework to work with the applicant in a positive and proactive manner based on seeking solutions to problems arising in dealing with planning applications in the following ways:

detailed advice in the form of statutory policies in the Core Strategy/ Unitary Development Plan, Supplementary Planning documents, and other written guidance has been made available;

a full pre application advice service has been offered;

where appropriate the City has been available to provide guidance on how outstanding planning concerns may be addressed.

- 2 The provisions of Part 3, Class E, of Schedule 2 to the Town & Country Planning (General Permitted Development) Order 1995 will permit changes of use between office (B1) and retail (A1) at part basement and part ground floor levels for a period of ten years from the date of this permission.

- 3 Access for people with disabilities is a material consideration in the determination of planning applications. The City of London Corporation has published design standards giving advice on access for people with disabilities and setting out the minimum standards it expects to see adopted in the City buildings. These can be obtained from the City's Access Adviser, City Planning Officer and District Surveyor. Further advice on improving access for people with disabilities can be obtained from the City's Access Adviser. Your attention is drawn to the Disability Discrimination provisions of the Equality Act 2010 to ensure that disabled people are not significantly disadvantaged.

Service providers, etc., should make "reasonable adjustments" to facilitate access to their premises and the City asks all applicants for planning permission to ensure that physical barriers to access premises are minimised in any works carried out.

- 4 This permission must in no way be deemed to prejudice any rights of light which may be enjoyed by the adjoining owners or occupiers under Common Law.

Blamires, Michael

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From: David Lonsdale <[REDACTED]>  
Sent: 14 January 2015 14:09  
To: Adjei, William  
Cc: Blamires, Michael  
Subject: Re: Walsingham House 35 Seething Lane, London EC3N 4AH

Dear Mr Adjei,

I have been twice consulted about the plans for Walsingham House and have studied the proposed scheme carefully. My flat is directly opposite the building at 41 Crutched Friars.

This is an example of how good architecture can enhance the appearance of an already distinguished building. The scale is appropriately grand for the site and will enable the owners to offer further and much better office accommodation for the City of London which is of course greatly needed.

I support the application.

Yours

David Lonsdale

On Wed, Jan 14, 2015 at 5:01 AM, Adjei, William <[William.Adjei@cityoflondon.gov.uk](mailto:William.Adjei@cityoflondon.gov.uk)> wrote:

> Please see attached consultation for the above mentioned address.

>

>

>

> Many Thanks

>

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## City of London Conservation Area Advisory Committee

Mr. Ted Rayment,  
Department of the Built Environment  
Corporation of London,  
P.O. Box 270,  
Guildhall,  
London, EC2P 2EJ

4<sup>th</sup> February 2015

Dear Sir,

At its meeting on 22<sup>nd</sup> January 2015 the City of London Conservation Area Advisory Committee considered the following planning application and reached the decision given below:

**C.6 14/01226/FULL - Walsingham House, 35 Seething Lane, London, EC3N 4AH  
Trinity Square Conservation Area/Tower Ward. Ward Club rep. Ward Club  
rep. Claire Cross**

External alterations including the replacement of the 7th floor, the addition of two storeys (office B1 use) with a plant room at roof level; infilling of the lightwell and the sloping roof fronting onto Crutched Friars; flexible office (B1)/retail (A1) use of part of the ground floor; ancillary roof plant and cycle spaces.

There were no objections and the Officers were requested to consult with the applicants about the possible retention of the ship that formed part of the existing timber doors.

I should be glad if you would bring the views of the Committee to the attention of the Planning & Transportation Committee.

Yours faithfully,



Mrs. Julie Fox  
Secretary

## Blamires, Michael

---

**From:** Jezeph, Alexandra [REDACTED]  
**Sent:** 02 June 2015 18:15  
**To:** Blamires, Michael  
**Subject:** FW: ROL7011 Walsingham House

Dear Michael,

Further to our recent conversations regarding the Walsingham House application, I write to forward a response from our rights to light consultant which addresses the points raised in the objection letter submitted by Mr Will.

I will give you a call tomorrow to discuss the note and any further information you may need in the lead up to committee.

Thanks and regards,  
Alex

Alexandra Jezeph  
Senior Planner  
30 Warwick Street | London W1B 5NH



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---

**From:** Lance Harris [mailto:[REDACTED]]  
**Sent:** None  
**To:** Jezeph, Alexandra  
**Cc:** Ferguson, Sharon; Festus Moffat ([REDACTED]); [REDACTED] Matthew Harris  
**Subject:** ROL7011 Walsingham House

Dear Mrs Jezeph

You recently forwarded me a copy of an email dated 14<sup>th</sup> May 2015 from Michael Will (owner of Flats 2 and 3, 41 Crutched Friars) to David Stothard and Michael Blamires of the City of London planning department, setting out his objections to the Walsingham House proposals.

As Mr Will's objections relate solely to the question of light, you have asked me to reply in this note, so I have responded below to the specific points raised and summarised the position with the Crutched Friars residential properties in terms of the overall position.

### Specific points of objection

1. Mr Will suggests that the proposed scheme breaches the terms of a deed dating back to 1929. The terms of the deed in question are yet to be debated with Mr Will and no agreement has therefore been reached as to how the deed applies to the current proposals. However, that is a separate, legal matter to be resolved between the parties and has nothing to do with the Town & Country Planning process. It is something which would fall outside of the expertise of the planning department and

not something which can play any part in the decision making process when considering the planning application.

2. Mr Will further objects on the basis that there will be unacceptable impacts on the daylight and sunlight received by his properties *"that are both unacceptable to me and non-compliant with BRE guidelines"*.

I think the starting point must be to emphasise the fact that we are dealing with guidelines rather than a set of rules to be applied rigidly in every case, so it is not a question of compliance or non-compliance, but rather an assessment of whether the impacts are reasonable in all the circumstances of any particular project and its context. Indeed, the BRE guide to which Mr Will refers goes out of its way to emphasise this in the introductory section from which the following quotes have been taken:-

- *"The advice given here is not mandatory and this document should not be seen as an instrument of planning policy; its aim is to help rather than constrain the designer"*
- *"Although it gives numerical guidelines, these should be interpreted flexibly since natural lighting is only one of many factors in site layout design"*
- *"Note that numerical values given here are purely advisory. Different criteria may be used, based upon the requirements for daylighting in an area viewed against other site layout constraints"*.
- *"Different targets may be used, based on the special requirements of the proposed development or its location"*.

What is clear from this is that rigid application of the numerical guidelines may well give rise to an inappropriate answer and form of development for city centre sites, in which case it may be appropriate to adopt lower target values that are more appropriate to the location concerned. No better example of that can be Walsingham House and its position in the City of London boundaries.

The Anstey Horne report dated 26<sup>th</sup> February 2015 acknowledges that there will be some transgressions of the BRE guidelines, if rigidly applied, at both 39-41 and 42 Crutched Friars, where there are already some lower existing daylight and sunlight levels because of the built up nature of the surrounding vicinity, in particular the Hilton Doubletree development alongside Walsingham House. As a result, these properties rely more heavily on the light being received from over the development site, and are therefore inevitably more sensitive to any increase in massing to the existing building profile.

Respecting the need to give full consideration to the impact on neighbouring properties, Anstey Horne was instructed to work closely with John Robertson Architects to assist them in producing a design that is sensitive to the existing surrounding buildings, whilst retaining the integrity of the current building on the site. Unlike the more dominant obstruction created by the Hilton Doubletree, the proposed scheme for Walsingham House discreetly sets back the upper floors from the main street frontage to minimise the impacts upon its neighbours, including the Crutched Friars residential properties that Mr Will owns.

As is always the case in central city locations, it is necessary to apply the guidelines with some common sense and flexibility in order to avoid built forms that are inappropriate for the location. The City of London's Environmental Quality Policy ENV35 refers to this flexible application of the BRE guidelines and in that context the general intentions of the City of London's planning policy on daylight and sunlight is therefore satisfied by the Walsingham House scheme.

Regards

Lance Harris  
Director



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**Department of the Built Environment**

Philip Everett BSc, CEng, MICE  
Director of the Built Environment

Jones Lang La Salle  
Attn: Miss Alexandra Jezeph  
[by email only]

Telephone 020 7332 1933  
Email [pippa.jackson@cityoflondon.gov.uk](mailto:pippa.jackson@cityoflondon.gov.uk)  
Typetalk: 18001 020 7332 1933

Date 20<sup>th</sup> January 2015

Dear Miss Jezeph

**Re: Walsingham House, 35 Seething Lane, London, EC3N 4AH**  
**Planning Application No: 14/01226/FULMAJ**

We have been consulted with regard to the above Planning application. Based upon the application documents submitted, we make the following observations:

- 1 It is pleasing that level access from the street has been provided at the entrance to this building.
- 2 Please confirm the size of the platform lift to be installed within the entrance lobby.
- 3 Confirmation is sought that all the internal steps will have handrails on either side which extend horizontally by 300mm beyond the top and bottom step. BS 8300 5.10.2 d & 5.10.1 and Building Regulations Part M 1.37 c & d
- 4 Each step nosing should incorporate a permanently contrasting continuous material for the full width of the stair on both the tread and the riser to help blind and partially sighted people appreciate the extent of the stair and identify individual treads. The material should be 50mm to 65 mm on the tread and 30 mm to 55 mm on the riser, and should contrast visually with the remainder of the tread and riser. BS 8300 5.9.5 Please ensure that all the internal steps meet this requirement.
- 5 The basement cycle facilities contain no provision for wheelchair accessible shower facilities. This needs to be rectified.
- 6 It is noted that all of the unisex wheelchair accessible WC compartments have sliding doors; justification for this should be set out in the access statement.
- 7 All the unisex wheelchair accessible WC's have concealed cisterns; this leads to boxing in and handrails not being as accessible as they should be. The back wall is now not flat but has a corner along it affecting access to the handrail on the transfer side of the WC pan. The reasoning for this design should be provided along with detail plans to ensure that the usability of the facility is not compromised.

City of London Access Team,  
Dept of the Built Environment  
P O Box 270, Guildhall, London EC2P 2EJ  
Switchboard 020 7606 3030  
[www.cityoflondon.gov.uk](http://www.cityoflondon.gov.uk)

[access@cityoflondon.gov.uk](mailto:access@cityoflondon.gov.uk)

- 8 **There are no ambulant disabled person's cubicles shown; please ensure that there is one ambulant disabled person's cubicle per group of facilities in addition to the wheelchair accessible unit.**
- 9 **Please confirm that level access to the terraces on floors 7, 8 and 9 will be provided.**
- 10 **It is noted that both the retail units have level changes to be overcome within the units. One unit proposes using a ramp adjacent to the entrance door. Please be aware that landings should be provided at the foot and head of a ramp. They should be at least the width of the ramp and not less than 1200 mm long, clear of any door swing or other obstruction. BS 8300 5.8.4 and Building Regulations Part M 1.26 h**

The above observations extend beyond issues relating to planning approval and are based on best practice standards for pan-disability access and inclusive design. We wish to draw your attention to these factors at this stage, to encourage early consideration of accessibility in the design process so that a truly inclusive environment can ultimately be achieved that everyone will be able to visit, use and enjoy.

We look forward to receiving your response regarding the above observations and to working with you to ensure that inclusive access for all will be provided.

Yours sincerely

Pippa Jackson PG Dip  
Access Adviser

c.c: Michael Blamires, Planning Services & Development Division

Hassall, Pam

---

From: PLN - Comments  
Subject: FW: Walsingham House - ROL7011 Walsingham House - OBJECTION

-----Original Message-----

From: Michael Will [mailto:  
Sent: 14 May 2015 13:47  
To: Stothard, David; Blamires, Michael  
Cc: Hampson, Annie  
Subject: Walsingham House - ROL7011 Walsingham House - OBJECTION

Dear Sirs,

I object to this application on the grounds that it reduces the levels of daylight and sunlight received by my 2 properties at Flat 2 and Flat 3 41 Crutched Friars, London EC3N 2AE to levels that are both unacceptable to me and non-compliant with BRE Guidelines.

This application also breaches the terms of a permissive deed executed in 1929 as the roof is above 120 feet ordinance datum.

In the unlikely event you wish to progress this application to the Planning and Transportation Committee kindly provide me a copy of the officer's report in support in advance in order that I may provide further written representations. Moreover, I hereby give notice that I may wish to make oral representations to the committee so that they understand first hand the damage this application will cause to my properties.

Kindly acknowledge receipt.

Yours faithfully,

Michael A. Will  
Solicitor



ENGLISH HERITAGE

LONDON OFFICE

Mr Michael Blamires  
Corporation of London  
Department of Planning & Transportation  
PO Box 270  
Guildhall  
LONDON  
EC2P 2EJ

Direct Dial: 020 7973 3775  
Direct Fax: 020 7973 3792

Our ref: W: P00444276

20 January 2015

Dear Mr Blamires

**Notifications under Circular 01/2001, Circular 06/2009 &  
T&CP (Development Management Procedure) Order 2010  
WALSINGHAM HOUSE 35 SEETHING LANE LONDON EC3N 4AH  
Application No 14/01226/FULMAJ**

Thank you for your letter of 14 January 2015 notifying English Heritage of the application for planning permission relating to the above site. Our specialist staff have considered the information received and we do not wish to offer any comments on this occasion.

**Recommendation**

**This application should be determined in accordance with national and local policy guidance, and on the basis of your specialist conservation advice.**

It is not necessary for us to be consulted again on this application. However, if you would like further advice, please contact us to explain your request. We can then let you know if we are able to help further and agree a timetable with you.

In returning the application to you without comment, English Heritage stresses that it is not expressing any views on the merits of the proposals which are the subject of the application.

Please note that this response relates to historic building and historic area matters only. If there are any archaeological implications to the proposals it is recommended



1 WATERHOUSE SQUARE 138-142 HOLBORN LONDON EC1N 2ST

Telephone 020 7973 3000 Facsimile 020 7973 3001  
[www.english-heritage.org.uk](http://www.english-heritage.org.uk)

*English Heritage is subject to the Freedom of Information Act, 2000 (FOIA) and Environmental Information Regulations 2004 (EIR). All information held by the organisation will be accessible in response to an information request, unless one of the exemptions in the FOIA or EIR applies.*





ENGLISH HERITAGE

LONDON OFFICE

that you contact the Greater London Archaeological Advisory Service for further advice (Tel: 020 7973 3712).

Yours sincerely



**Tom Nancollas**  
Business Officer

E-mail: [thomas.nancollas@english-heritage.org.uk](mailto:thomas.nancollas@english-heritage.org.uk)



1 WATERHOUSE SQUARE 138-142 HOLBORN LONDON EC1N 2ST

Telephone 020 7973 3000 Facsimile 020 7973 3001  
[www.english-heritage.org.uk](http://www.english-heritage.org.uk)

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**Rights of Light and Party Wall Consultants**  
4 Chiswell Street, London EC1Y 4UJ  
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F 020 7065 2779  
www.ansteyhorne.co.uk

Chartered Surveyors



Our ref: LJH/SB/ROL7011

David Stothard  
Assistant Director, Development Division  
Department of the Built Environment  
City of London  
Guildhall  
PO Box 270  
London  
EC2P 2EJ

3 March 2015  
By email

Dear Mr Stothard

Re: (ROL7011) Walsingham House

As you know, Anstey Horne is advising on daylight and sunlight issues in connection with the Walsingham House scheme.

I am sorry that my unexpected absence meant that our previously planned meeting had to be cancelled. I do think there is merit in meeting so that we can talk through the issues and any concerns you have in detail, but in the meantime I have set out below a response to the points raised in your email to Alexandra Jezeph of February 12<sup>th</sup>.

1. I genuinely do not believe it would be appropriate to look at this on a cumulative basis, taking account of the impacts of the Hilton Doubletree development, for the following reasons:-

- a) Although we do sometimes produce cumulative impact assessments for EIAs on larger schemes, that is always where the proposed scheme is coming forward at the same time as other schemes in the vicinity, such that the planners understandably want to understand the total impact on the neighbour. Obviously we do not have that situation here because the Hilton Doubletree works were completed several years ago, and I honestly cannot remember ever being asked to produce a cumulative study in relation to works completed so far in the past.

Contd:

Directors: Lance Harris MRICS c. Graham North FRICS MCIArb c. Heather Schöpp BSc (Hons) MRICS c. Aidan Cosgrave BSc (Hons) MRICS  
s. Paul Goldsmith BSc (Hons) MRICS c. Matthew Craske BA (Hons)

Associate Directors: Nick Bury c. Matthew Briggs BEng (Hons) PGDip c. Sally Tester BSc (Hons) MRICS c. Matthew Harris BSc (Hons)  
c. Drew Steadman BA (Hons) c. Tom Bentley BSc (Hons)

Anstey Horne is the trading name of Anstey Horne & Co Ltd, a company registered in England and Wales, number 554355.  
Registered office: 4 Chiswell Street, London EC1Y 4UJ

Regulated by RICS

- b) Your Local Plan does refer to taking account of cumulative effects, but for the reasons described in point a) above I do not think that is applicable here. Also, what the BRE guide says is that where there are successive works of extension or alteration to a particular building one should look at the cumulative effects, so that, in effect, the developing party cannot chip away at the light enjoyed by the neighbour by virtue of a succession of alterations. That does not apply here either.
  - c) The Walsingham House scheme is already been penalised here by virtue of the past impacts created by the Hilton Doubletree, because the light to 39 and 41 Crutched Friars was materially affected by that scheme. The Walsingham House scheme therefore has to cope with the fact that there is relatively limited light available to the Crutched Friars properties, which is why it has been designed in such a sensitive fashion, raking back sharply away from Crutched Friars rather than raising in a more vertical fashion as was the case with the Hilton Doubletree scheme.
  - d) On a practical level it creates problems as well because we would have to research what used to exist on the Hilton Doubletree site and model and test a baseline from there, information we do not currently have and which might be difficult to obtain in truly accurate form.
2. There are minor discrepancies between our figures for the existing condition and the figures GIA calculated for the proposed condition. However, I have no idea what level of information GIA were provided with for their analysis, whereas our client paid for a detailed and accurate survey of the site and all the surrounding buildings, so I believe our results to be entirely reliable and the ones we should be looking at. You point out that there were discrepancies between the results in the GIA reports of 1998 and 2006, so there is an obvious problem if one refers back to past work undertaken by other parties, over which we have no control and which might have been based upon some degree of estimation.
  3. We have not used the test of average daylight factor, because it is not in that part of the BRE guide that deals with the impact upon existing neighbours. As you know, it is a design tool for new residential development and paragraph F7 of Appendix F specifically says "*use of the ADF for loss of light to existing building is not generally recommended*". It does go on to give examples where exceptions might be made, but none of those examples fit with our circumstance.

The vertical sky component, daylight distribution and APSH tests are recommended because they are comparative, allowing everyone to see the difference between the existing and proposed conditions. The ADF test is not designed to be comparative, it is simply looking at light conditions alongside the interior daylighting recommendations for new residential development.

I hope this explanation is useful, but I still think it might be a good idea to get together for a short meeting to talk through the relevant points so that you are entirely comfortable. Probably like you, I have quite a congested diary most weeks, but I will do my very best to fit in with your schedule and our offices are but a few minutes apart.

In the meantime we have produced the attached amended version of our initial daylight/sunlight report to take account of clarification previously raised by Michael Blamires, particularly in respect of 39/41 Crutched Friars. For example, we have now separated the results out on a flat by flat basis. It also updates the results for Mr Will's flats and one other, where we have now had access, but the results are almost identical.

I look forward to hearing from you.

Yours sincerely



Lance J Harris

PS. A hard copy of the updated report will be hand delivered tomorrow.

cc. Michael Blamires  
Annie Hampson  
Alexander Jezeph  
Sharon Ferguson  
Matthew Harris

<b>Committee:</b>	<b>Date:</b>
Planning and Transportation	23 June 2015
<b>Subject:</b> Opposite Rising Sun Court Long Lane London EC1A 9EJ Replacement of public payphone kiosk with combined public payphone and ATM booth and associated change of use.	<b>Public</b>
<b>Ward:</b> Farringdon Within	<b>For Decision</b>
<b>Registered No:</b> 15/00426/FULL	<b>Registered on:</b> 12 May 2015
<b>Conservation Area:</b> Smithfield	<b>Listed Building:</b> No

## Summary

The application relates to a single telephone box located within the public footway on the south east side of Long Lane opposite Smithfield East Market and in front of Rising Sun Court, a pedestrian thoroughfare which spurs off Long Lane between Nos. 56 and 58 Long Lane.

The telephone box lies within Smithfield Conservation Area, is of contemporary design and is considered not to be a heritage asset.

Planning permission is sought to replace the existing telephone box with a combined public payphone and ATM cashpoint box and to approve an associated change of use from sui-generis to Class A2 (financial and professional services). The replacement box would be of solid construction utilising a modified type K6 BT box design and would have both payphone and ATM machines located externally. The footprint of the proposed box would increase from 0.9m x 0.9m to 0.9m x 1.0m.

The applicant has maintained that the site falls within an area which has been identified as having a high demand for ATM facilities.

It is considered that the proposed change of use would introduce an inappropriate activity into the public domain resulting in a loss of visual permeability arising from the solidification of the structure. The proposal would increase the incidence of street 'clutter', detracting from the appearance of the street scene within Long Lane and would fail to preserve or enhance the character of Smithfield Conservation Area.

The City's streets are subject to increasingly high levels of footfall and improvements to pedestrian permeability and enhancement of the public realm is consequently a priority for the City. It is considered that the development would unnecessarily prejudice existing and future free pedestrian movement within the footway.

The resulting level of harm would not be mitigated by any perceived economic or social benefit.

### **Recommendation**

That the application be refused for the reasons set out in the attached schedule.

# Site Location Plan



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ADDRESS:  
Opposite Rising Sun Court, Long Lane

CASE No.  
15/00426/FULL

-  SITE LOCATION
-  LISTED BUILDINGS
-  CONSERVATION AREA BOUNDARY



**CITY  
OF  
LONDON**

DEPARTMENT OF THE BUILT ENVIRONMENT



Images of BT box for 15/00426/FULL



## **Main Report**

### **Site**

1. The application relates to an isolated telephone box that is located within the public footway on the south east side of Long Lane opposite Smithfield East Market, a Grade 2\* listed building, and in front of Rising Sun Court, a pedestrian thoroughfare which spurs off Long Lane between Nos. 56 and 58 Long Lane, and which affords character views of St Bartholomew the Great Church a Grade 1 listed building to the south, and connects to a Grade 2 listed building fronting Cloth Fair.
2. The telephone box lies within Smithfield Conservation Area, and is of contemporary design. The box is considered not to be a heritage asset.

### **Proposal**

3. Planning permission is sought for the change of use from a telephone payphone box which is classed as sui-generis, to a mixed use comprising a payphone and an ATM which falls within Class A2 (financial and professional services) of the Town and Country (Use Classes) Order 1987 (as amended).
4. The change of use would involve disposal of the existing telephone box and erection of a traditional type K6 BT telephone box design which would be substantially modified. The box would be sealed with no access to the internal space and would have payphone and ATM machines located externally.
5. The resulting conversion would dispense with clear glazing giving rise to a visually solid form. The footprint of the proposed box would increase from 0.9m x 0.9m to 0.9m x 1.0m.

### **Consultations**

6. The application has been publicised on site and in the press.
7. The views of other City of London departments have been taken into account in the consideration of this scheme.
8. The City of London Conservation Area Advisory Committee (CAAC) support the City's policy of seeking to reduce street clutter and has objected to the proposal considering it to be detrimental to the street scene and to the conservation area.

### **Policy Context**

9. The development plan consists of the London Plan and the City of London Local Plan. The London Plan and Local Plan policies and Supplementary Planning Documents that are most relevant to the consideration of this case are set out in Appendix A to this report. It is necessary to assess all of the policies and proposals in the Development Plan and to come to a view as to whether the proposal satisfies the requirements of the Plan.

10. Government planning guidance is contained within the National Planning Policy Framework (NPPF) and accompanying National Planning Practice Guidance (NPPG).
11. Although not a Development Plan Document, the City of London Corporation City Street Scene Manual is considered material as it provides specific guidance for developers in respect of telephone boxes and kiosks that are located within the highway.

### **Considerations**

12. The Corporation in determining the planning application has the following main statutory duties to perform:-
  - To have regard to the provisions of the development plan, in so far as it is material to the application, to local financial considerations so far as they are material to the application, and to any other material considerations (Section 70 (2) Town & Country Planning Act 1990);
  - To determine the application in accordance with the development plan unless other material considerations indicate otherwise (Section 38(6) of the Planning and Compulsory Purchase Act 2004);
  - In considering whether to grant planning permission for development which affects a listed building or its setting, to have special regard to the desirability of preserving the building/structure or its setting or any features of special architectural or historic interest which it may possess. (S66 (1) Planning (Listed Building and Conservation Areas) Act 1990)
  - When considering the applications, special attention shall be paid to the desirability of preserving or enhancing the character or appearance of the conservation area (S72 (1) Planning (Listed Buildings and Conservation Areas) Act 1990).
13. Chapter 12 of the NPPF is relevant in this instance as it sets out key policy considerations for applications relating to designated and non-designated heritage assets. Other relevant guidance is provided by Historic England including the document's Conservation Principles, and The Setting of Heritage Assets together with Building in Context (HE/CABE) and the PPS5 Practice Guide in respect of the setting of heritage assets.
14. Considerable importance and weight should be given to the desirability of preserving or enhancing the character or appearance of a conservation area and the setting of listed buildings, when carrying out any balancing exercise in which harm to the significance of conservation areas or the setting of listed buildings is to be weighed against public benefits. A finding that harm would be caused to a conservation area or the setting of a listed building gives rise to a strong presumption against planning permission being granted.

15. The principal issues in considering this planning application are:
- The extent to which the proposal complies with Government policy advice (NPPF) and the relevant policies of the Development Plan, having particular regard to:
  - The acceptability of the proposed alterations in design and heritage terms.
  - The suitability of the site and its location

### **The Acceptability of the Proposal in Design and Heritage Terms**

16. Policy DM12.2 of the Local Plan states that development in conservation areas will only be permitted if it preserves and enhances the character or appearance of the conservation area. Policy DM12.1 seeks to ensure that the significance of heritage assets is sustained. Policy DM10.1 encourages a high standard of design in development proposals. Policy 7.8 of the London Plan seeks to ensure that development affecting heritage assets and their setting should conserve their significance by being sympathetic to their form, scale, materials and architectural detail. Chapters 7 and 12 of the NPPF set out relevant design and heritage policies.
17. In this instance consideration needs to be given to the impact that the proposal would have on the significance of the Smithfield Conservation Area as a designated heritage asset. The existing telephone box is non-designated and is considered not to be a heritage asset. It is of contemporary design and appearance and although comprising an identifiable and commonplace element of street architecture does not contribute positively to the appearance or character of the conservation area. Accordingly the loss of the telephone box would not be detrimental to the conservation area. Although Conservation Area Consent is not required for its removal, and a telecom replacement would be subject to a deemed consent process, planning permission would be required where there would be a change of use.
18. The proposed development would lie directly opposite and within the wider setting of Smithfield East Market a large Grade 2\* listed building. It also lies on a pedestrian approach to Rising Sun Court a Grade 2 listed building fronting Cloth Fair to the south east and adjoins an important local viewing point for the church of St Bartholomew the Great.
19. In the case of the impact upon the setting of Smithfield Market and the Grade 2 listed building within Rising Sun Court, it is considered that as a result of the separation distances and the scale of the proposed development, there would be less than significant harm to the setting of these listed buildings.
20. However Smithfield Conservation Area Character Summary and Management Strategy SPD (2.26MB) adopted 18 September 2012 records a significant public viewing point (View 8) adjacent to the existing telephone box, of the north transept of St Bartholomew-the-Great as seen from Long Lane through Rising Sun Court. The

solidification of the telephone box in conjunction with the likelihood that existing views would be further compromised by the potential assembly of persons using the box, would obstruct this important local view of the church of St Bartholomew-the Great, further detracting from the visual amenity of the locality and resulting in less than substantial harm to the significance of this part of Smithfield Conservation Area as a designated heritage asset.

21. A key characteristic of the existing telephone box is that the glazing is transparent and allows light and public views to permeate through the structure. The proposed replacement structure would be materially different, having no clear glazing and as a result appearing as a solid structure within the street scene. Notwithstanding the use of a more traditional K6 telephone box for the proposed combination payphone/ATM kiosk, the resulting conversion would no longer possess the design aesthetic or historic significance of that iconic structure and would consequently not deliver any enhancement to the character of the conservation area.
22. The proposal would require persons using the kiosk to stand within the footway extending the use of the site beyond its built footprint and further reducing the 2m 'pinch point' between the development and the nearest building.
23. Consideration has been given to paragraph 134 of the NPPF. It is considered that the less than substantial harm to the conservation area would not be outweighed by the public benefits of the proposal. The proposals would therefore be contrary to policies DM 12.2, DM12.1 and DM10.1 of the Local Plan, policy 7.8 of the London Plan and the aims of chapters 7 and 12 of the NPPF.
24. The Corporation's City Street Scene Manual recognizes that telecommunication kiosks have an important role to play within City streets and states that whilst providing a valuable amenity within the public realm they can also contribute positively to the street scene and act as a visual reference point to people unfamiliar with an area. Whilst every effort should be made to make telephone boxes accessible to people with disabilities, where the boxes are redundant and contribute to clutter, the manual confirms that the City will actively pursue their removal and will welcome the removal of unprofitable modern call boxes. The manual further recognizes that retail or other forms of kiosk as distinct from telephone boxes are not a common feature within the City's streets due to the general lack of space on the existing walkways and public spaces. Only (non-telecom) kiosks of high quality design that would not detract from the surrounding streetscape, would not obscure key views within the streetscape, would not compromise circulation in the footway or obstruct pedestrian flows, and would provide accessibility for disabled persons, would be acceptable. In this instance the proposal does not satisfy such criteria.

### **The Suitability of the Site**

25. Policy CS20 of the Local Plan seeks to focus new retail development (including A2 uses) within the Principal Shopping Centres and encourage movements between the principal Shopping Centres by enhancing the retail environment in the retail links. The site is not within a Principal Shopping Centre or a Retail Link as defined by the Local Plan. It does however lie alongside a shopping parade that extends for some length along this part of Long Lane.
26. Policy DM10.4 of the Local Plan encourages the enhancement of highways, the public realm and other spaces. It states that enhancement schemes should be of a high standard of design, having regard to the following matters of relevance to the determination of this application:
  - Connections between spaces and the provision of pleasant walking routes;
  - The need to provide accessible and inclusive design, ensuring that streets and walkways remain uncluttered;
  - The need for pedestrian priority and enhanced permeability.
27. Policy CS16 of the Local Plan aims to improve conditions for safe and convenient walking. London Plan Policy 6.10B states that development proposals should ensure high quality pedestrian environments. London Plan policy 7.5B advises that street furniture and infrastructure should be of the highest quality, maintain uncluttered spaces and should contribute to easy movement of people through space.
28. The City's streets currently have a high level of footfall particularly during peak hours. A report was presented to the Planning and Transportation Committee on the 13<sup>th</sup> January 2015 regarding items on the highway ('A' boards, bike racks etc.). The report noted that the City is expecting a significant increase in commuters, shoppers and visitors. The current daily population of users of the City is estimated to be around 330,000 people and with the growth of the 'Eastern Cluster' office developments, the construction of Crossrail, Bank upgrade and the Thameslink upgrade the City's daily population is predicted to rise to well over 400,000 in the next ten years. This could result in the streets becoming even busier. The London Plan reinforces the importance of planning for growth (e.g. "Context and Strategy" paragraph 1.47).
29. The application site is in close proximity to Smithfield Market and forms part of an east west route between Aldersgate and the market. The area has high levels of footfall not only during commuter and lunchtime periods but during those times associated with the operation of Smithfield Market.
30. The telephone box also lies in close proximity to the entrance of the Farringdon East Crossrail Station currently under construction which when completed would result in a significant increase in pedestrian footfall in the area.

31. The existing telephone box already forms a pinch point on the footway. The proposed mixed payphone and ATM use would spill onto the highway to such a degree that it could result in obstruction and detract from the permeability of the locality.
32. The clutter and highway obstruction that the proposed use would generate would detract from the public realm and pedestrian permeability contrary to the aims of policies DM10.4 and DM17.1 of the Local Plan and policies 6.10B and 7.5B of the London Plan.
33. Ease of pedestrian movement and the enhancement of the public realm is a priority for the City and in many instances there would be a preference for most non-listed telephone boxes unless regarded as non-designated heritage assets that are no longer required for telecommunication purposes to be removed from the highway.
34. It is acknowledged that accessibility to the payphone would be improved for disabled persons. However improved payphone accessibility could be achieved by alteration to the existing telephone box facility in accordance with the provisions of Part 16 of Schedule 2 to the Town and Country Planning (General Permitted Development) (England) Order 2015. Provision of an ATM is not necessary in order to improve accessibility to the payphone.

### **Conclusion**

35. It is considered that the solidified appearance of the replacement structure would obstruct an important view of the Listed Grade 1 church to the south, would clutter the street and fail to preserve or enhance Smithfield Conservation Area resulting in less than substantial harm to the significance of this part of the Conservation Area as a designated heritage asset.
36. Within the City it is projected that footfall will increase further over the next 10 years as a result of natural growth and improved public transport services including the new Crossrail Farringdon East station. The proposed conversion of the telephone box would obstruct the highway to an unacceptable degree
37. Although the proposal would provide a more accessible payphone and the provision of an ATM such benefits would not outweigh the harm that has been identified. For these reasons the proposal is considered to be contrary to policies DM 12.1, DM12.2, DM10.1 and DM10.4 of the Local Plan, policies 6.10A/B, 7.5A/B and 7.8D of the London Plan and the aims of chapters 7 and 12 of the NPPF.

## **Background Papers**

### Internal

29.05.2015 City Transportation - Memo

### External

Design and Access Statement

Design and Security Statement

Visual of converted telephone box

Standard KX100 telephone kiosk as existing: Drawing number - T2

Photographs of the site as existing

02.06.2015 City of London Conservation Area Advisory Committee - Memo

## **Appendix A**

### **London Plan Policies**

Policy 6.10 Development proposals should ensure high quality pedestrian environments and emphasise the quality of the pedestrian and street space.

Policy 7.5 Development should make the public realm comprehensible at a human scale. Landscape treatment, street furniture and infrastructure should be of the highest quality, have a clear purpose, maintain uncluttered spaces and should contribute to the easy movement of people through the space.

Policy 7.8 Development should identify value, conserve, restore, re-use and incorporate heritage assets, conserve the significance of heritage assets and their settings and make provision for the protection of archaeological resources, landscapes and significant memorials.

### **Relevant Local Plan Policies**

#### ***CS3 Ensure security from crime/terrorism***

To ensure that the City is secure from crime, disorder and terrorism, has safety systems of transport and is designed and managed to satisfactorily accommodate large numbers of people, thereby increasing public and corporate confidence in the City's role as the world's leading international financial and business centre.

#### ***CS10 Promote high quality environment***

To promote a high standard and sustainable design of buildings, streets and spaces, having regard to their surroundings and the character of the City and creating an inclusive and attractive environment.

#### ***DM10.1 New development***

To require all developments, including alterations and extensions to existing buildings, to be of a high standard of design and to avoid harm to the townscape and public realm, by ensuring that:

- a) the bulk and massing of schemes are appropriate in relation to their surroundings and have due regard to the general scale, height, building lines, character, historic interest and significance, urban grain and materials of the locality and relate well to the character of streets, squares, lanes, alleys and passageways;
- b) all development is of a high standard of design and architectural detail with elevations that have an appropriate depth and quality of modelling;
- c) appropriate, high quality and durable materials are used;
- d) the design and materials avoid unacceptable wind impacts at street level or intrusive solar glare impacts on the surrounding townscape and public realm;



- e) development has attractive and visually interesting street level elevations, providing active frontages wherever possible to maintain or enhance the vitality of the City's streets;
- f) the design of the roof is visually integrated into the overall design of the building when seen from both street level views and higher level viewpoints;
- g) plant and building services equipment are fully screened from view and integrated in to the design of the building. Installations that would adversely affect the character, appearance or amenities of the buildings or area will be resisted;
- h) servicing entrances are designed to minimise their effects on the appearance of the building and street scene and are fully integrated into the building's design;
- i) there is provision of appropriate hard and soft landscaping, including appropriate boundary treatments;
- j) the external illumination of buildings is carefully designed to ensure visual sensitivity, minimal energy use and light pollution, and the discreet integration of light fittings into the building design;
- k) there is provision of amenity space, where appropriate;
- l) there is the highest standard of accessible and inclusive design.

#### ***DM12.1 Change affecting heritage assets***

1. To sustain and enhance heritage assets, their settings and significance.
2. Development proposals, including proposals for telecommunications infrastructure, that have an effect upon heritage assets, including their settings, should be accompanied by supporting information to assess and evaluate the significance of heritage assets and the degree of impact caused by the development.
3. The loss of routes and spaces that contribute to the character and historic interest of the City will be resisted.
4. Development will be required to respect the significance, character, scale and amenities of surrounding heritage assets and spaces and their settings.
5. Proposals for sustainable development, including the incorporation of climate change adaptation measures, must be sensitive to heritage assets.

#### ***DM10.4 Environmental enhancement***

The City Corporation will work in partnership with developers, Transport for London and other organisations to design and implement schemes for the enhancement of highways, the public realm and other spaces. Enhancement schemes should be of a high standard of design, sustainability, surface treatment and landscaping, having regard to:

- a) the predominant use of the space, surrounding buildings and adjacent spaces;
- b) connections between spaces and the provision of pleasant walking routes;
- c) the use of natural materials, avoiding an excessive range and harmonising with the surroundings of the scheme and materials used throughout the City;
- d) the inclusion of trees and soft landscaping and the promotion of biodiversity, where feasible linking up existing green spaces and routes to provide green corridors;
- e) the City's heritage, retaining and identifying features that contribute positively to the character and appearance of the City;
- f) sustainable drainage, where feasible, co-ordinating the design with adjacent buildings in order to implement rainwater recycling;
- g) the need to provide accessible and inclusive design, ensuring that streets and walkways remain uncluttered;
- h) the need for pedestrian priority and enhanced permeability, minimising the conflict between pedestrians and cyclists;
- i) the need to resist the loss of routes and spaces that enhance the City's function, character and historic interest;
- j) the use of high quality street furniture to enhance and delineate the public realm;
- k) lighting which should be sensitively co-ordinated with the design of the scheme.

#### ***DM10.8 Access and inclusive design***

To achieve an environment that meets the highest standards of accessibility and inclusive design in all developments (both new and refurbished), open spaces and streets, ensuring that the City of London is:

- a) inclusive and safe for all who wish to use it, regardless of disability, age, gender, ethnicity, faith or economic circumstance;
- b) convenient and welcoming with no disabling barriers, ensuring that everyone can experience independence without undue effort, separation or special treatment;
- c) responsive to the needs of all users who visit, work or live in the City, whilst recognising that one solution might not work for all.

#### ***CS12 Conserve or enhance heritage assets***

To conserve or enhance the significance of the City's heritage assets and their settings, and provide an attractive environment for the City's communities and visitors.

#### ***DM12.1 Change affecting heritage assets***

1. To sustain and enhance heritage assets, their settings and significance.
2. Development proposals, including proposals for telecommunications infrastructure, that have an effect upon heritage assets, including their settings, should be accompanied by supporting information to assess and evaluate the significance of heritage assets and the degree of impact caused by the development.
3. The loss of routes and spaces that contribute to the character and historic interest of the City will be resisted.
4. Development will be required to respect the significance, character, scale and amenities of surrounding heritage assets and spaces and their settings.
5. Proposals for sustainable development, including the incorporation of climate change adaptation measures, must be sensitive to heritage assets.

#### ***DM12.2 Development in conservation areas***

1. Development in conservation areas will only be permitted if it preserves and enhances the character or appearance of the conservation area.
2. The loss of heritage assets that make a positive contribution to the character or appearance of a conservation area will be resisted.
3. Where permission is granted for the demolition of a building in a conservation area, conditions will be imposed preventing demolition commencing prior to the approval of detailed plans of any replacement building, and ensuring that the developer has secured the implementation of the construction of the replacement building.

## SCHEDULE

APPLICATION: **15/00426/FULL**

**Opposite Rising Sun Court Long Lane London**

**Replacement of public payphone kiosk with combined public payphone and ATM booth and associated change of use.**

### REASONS FOR REFUSAL

- 1 The proposal would result in the erection of a visually impermeable structure which in conjunction with and exacerbated by the nature of the proposed use would prejudice important public views of St Bartholomew the Great Church an important Grade 1 listed building to the south, would detract from the appearance of the street scene, and add to street 'clutter' which would fail to preserve or enhance the character and appearance of this part of Smithfield Conservation Area contrary to the following Local Plan policies: CS10, DM10.1, CS12, DM12.1, DM12.2, DM12.3.
- 2 The proposed structure & change of use and the activity associated therewith would obstruct the public footway and pedestrian environment and would be an impediment to the free flow of pedestrian movements along this part of Long Lane contrary to the following Local Plan policies: CS10, DM10.1, CS16, DM16.1, DM16.2.

### INFORMATIVES

- 1 In dealing with this application the City has implemented the requirements of the National Planning Policy Framework to work with the applicant in a positive and proactive manner based on seeking solutions to problems arising in dealing with planning applications in the following ways:

detailed advice in the form of statutory policies in the Local Plan, Supplementary Planning documents, and other written guidance has been made available;

a full pre application advice service has been offered;

where appropriate the City has been available to provide guidance on how outstanding planning concerns may be addressed.

However, notwithstanding the above, it has not been possible to achieve solutions to the problems as the proposals are contrary to

planning policies, do not demonstrate other over-riding material considerations, and negotiations could not overcome the problems.

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<b>Committee</b>	<b>Dated:</b>
Planning & Transportation	23/06/2015
<b>Subject:</b> Adoption of the City of London Thames Strategy Supplementary Planning Document	<b>Public</b>
<b>Report of:</b> Director of the Built Environment	<b>For Decision</b>

## Summary

This report sets out the background and preparation process for the proposed adoption version of the City of London Thames Strategy Supplementary Planning Document (SPD), attached at Appendix 2. The purpose of this Strategy is to provide guidance on the interpretation and application of Local Plan Policy CS9 *Thames and the Riverside*.

The London Plan requires Thames-side local authorities to identify a Thames Policy Area and formulate specific policies and a strategy for this area. The adopted SPD will, alongside Local Plan Policy CS9, enable the City Corporation to meet this requirement.

The Thames Strategy SPD will replace the existing Riverside Appraisal Supplementary Planning Guidance which was adopted by the City Corporation in 2002.

The draft Thames Strategy SPD has been subject to public consultation and has been revised to take account of relevant comments. Changes include strengthening protection for historic assets, clarification of conditions for access to the foreshore, reiteration of the requirement that any development on or over the river should be for river related uses, updating of Thames Tideway Tunnel references and acknowledgement of links with protected views and public realm in neighbouring boroughs.

## Recommendation

Members are recommended to:

- Adopt Appendix 2 as the City of London Thames Strategy Supplementary Planning Document (June 2015).

## **Main Report**

### **Background**

1. On 13<sup>th</sup> January 2015, the Planning and Transportation Committee approved the draft Thames Strategy Supplementary Planning Document (SPD) for public consultation. This consultation took place between 18<sup>th</sup> March and 30<sup>th</sup> April 2015.
2. Comments were received from 17 respondents, providing 80 individual comments. The Thames Strategy Consultation Statement (Appendix 1) details the representations made and the City Corporation's proposed response to these comments.
3. The Thames Strategy SPD has been redrafted to take account of these comments, where appropriate.

### **Current Position**

4. The redrafted Thames Strategy SPD (Appendix 2) is presented for approval for adoption.
5. The main changes as a result of the public consultation are:
  - Strengthening of references to historic assets and their protection.
  - Clarification of the conditions and dangers associated with access to the foreshore and the explanation of relevant permits and river works licences that are required.
  - Reiteration that development on or over the river must require a riverside location for a river related use.
  - Updating of references to the development consent for the Thames Tideway Tunnel.
6. Additional changes have been made under the Duty to Co-operate with neighbouring boroughs:
  - Inclusion of reference to views originating in Westminster which take in the City's part of the riverside and riverside townscapes.
  - Reference to integration of the City's riverside walk with the Thames Path and public realm associated with the Tower of London in the London Borough of Tower Hamlets.
7. The draft SPD is accompanied by a Consultation Statement (Appendix 1), a Strategic Environmental Assessment Screening Report (Appendix 3) and an Equalities Impact Assessment Screening Report (Appendix 4).

### **Proposals**

8. Members are requested to approve the proposed changes as outlined in paragraphs 5 and 6 along with other minor clarification and updating changes. All the proposed changes can be viewed as tracked changes in Appendix 2.

### **Corporate & Strategic Implications**

9. The Thames Strategy SPD will assist the City Corporation in meeting the following key aims of the Corporate Plan 2015-19:



- KPP4 Maximising the opportunities and benefits afforded by our role in supporting London's communities.
  - KPP5 Increasing the outreach and impact of the City's cultural, heritage and leisure contribution to the life of London and the nation.
10. The Thames Strategy SPD will assist the City Corporation to fulfil the key objectives of the Planning & Transportation Departmental Business Plan, particularly objective III: "To provide a vision for a smarter future City and set out the key steps needed to deliver".
  11. The Thames Strategy SPD fulfils the requirements of Policy 7.29 of the London Plan which requires the City Corporation to formulate a strategy for the Thames Policy Area within its boundary and co-operate with neighbouring boroughs in its preparation.

### **Implications**

12. The Thames Strategy SPD provides guidance for applicants on the implementation of the adopted Local Plan policies for the Thames Policy Area. As such it does not create new policy and there are no financial or human resources implications associated with the adoption of this SPD.
13. Once adopted the SPD will supersede the "Riverside Appraisal of the Thames Policy Area in the City of London Supplementary Planning Guidance" April 2002.
14. The Thames Strategy SPD provides further guidance for applicants on the City's policy intentions for land uses and development in the Thames Policy Area. In parallel with this the City Corporation has adopted a Riverside Walk Enhancement Strategy which outlines the public realm improvements that the City Corporation intends to implement through its environmental enhancement programmes.

### **Conclusion**

15. The City of London Thames Strategy SPD provides guidance on the implementation of the City Corporation's adopted planning policies for the Thames Riverside in the City. Members are recommended to approve the SPD for adoption.

### **Appendices**

- Appendix 1 – Thames Strategy Consultation Statement
- Appendix 2 – draft Thames Strategy SPD for adoption
- Appendix 3 – Strategic Environmental Assessment Screening Report
- Appendix 4 – Equalities Impact Assessment Screening Report.

## **Background Papers**

- 1) Riverside Walk Enhancement Strategy – Adoption of Strategy. Report to Planning & Transportation Committee, 11th November 2014
- 2) The City of London Thames Strategy–draft Supplementary Planning Document – public consultation. Report to Planning & Transportation Committee, 13<sup>th</sup> January 2015

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**City of London  
Thames Strategy  
Supplementary Planning Document  
Consultation Statement  
June 2015**



## Introduction

The City of London Corporation is preparing a Thames Strategy Supplementary Planning Document (SPD) to provide guidance on the application of Local Plan Policies for the Thames Policy Area.

## Consultation requirements

Consultation on the draft City of London Thames Strategy SPD took place between the 18<sup>th</sup> March and 30<sup>th</sup> April 2015. The following stakeholder groups were consulted:

- Statutory consultees as identified in the City of London Statement of Community Involvement
- Stakeholders who have expressed an interest in planning policy and are included in the Development Plans consultation database.
- City of London Members ( Planning &Transportation Committee January 2015)
- Key officers from City of London Corporation Departments

Under the Duty to Co-operate the following organisations were approached for comment : Mayor of London, City of Westminster, LB Tower Hamlets, LB Southwark, LB Lambeth, LB Bexley, Environment Agency, Historic England, Natural England, Marine Management Organisation, Highways Agency, City & Hackney Clinical Commissioning Group, NHS England, Civil Aviation Authority, London Enterprise Panel, Office of Rail Regulation, Local Nature Partnership, Port of London Authority, Thames Water, Inner Temple (Honourable Society), Middle Temple (Honourable Society), Bankside Neighbourhood Forum, Bermondsey Neighbourhood Forum.

The draft City of London Thames Strategy SPD was made available on the City's web site. Printed copies of the City of London Thames Strategy SPD were made available at the Department of the Built Environment Enquiries Desk and the City's five libraries, during their normal opening hours.

## Responses

The consultation period triggered approximately 80 comments from 17 respondents. All representations were reviewed and appropriate changes made to the Thames Strategy. Table 1 shows the comments received and the City Corporation's response to each comment.

**Table 1 Comments received in response to the public consultation on the draft Thames Strategy SPD.**

Rep	Name	Organisation	Comment Highlighting identifies the main point of each comment All comments have been reproduced in full except where ** indicates that a comment has been summarised.	City of London Response ( <b>bold</b> text indicates change <del>strike through</del> denotes deletion and <u>underlining</u> denotes additional text)
1/1	Keith Bottomley	N/A	I have lived very close to the river Thames for 18 years and have worked in the City for the same length of time. I am planning to put my name forward for the forthcoming by-election in Bridge Ward and <b>have taken the opportunity, as a local resident, to seek the views of people who have a particular connection with that Ward and also more general views about the riverside.</b> I hope that these views are useful and make a constructive contribution as part of the public consultation.	Comment noted <b>No change needed</b>
1/2	Keith Bottomley	N/A	<i>What particular places and qualities make the river special to you?</i> Its openness, history, pedestrian walkways and the changes of character from section to section are seen as making the river special. The whole length of the riverside walk is the quintessential hallmark of the City of London. Having good views is seen as particularly important. People appreciate the interface between maintaining the history of the river and the need in a modern society to provide facilities that allow for progression and technological development. The mixture of historic and modern buildings provide a unique setting along the river but <b>care needs to be taken to ensure any new buildings do not compromise the look of the City and detract from its historical importance.</b>	<b>Additional Bullet Point added at 3.3</b> <b><u>• Opportunities for conservation of heritage assets, ensuring that new development does not detract from these assets and their settings.</u></b>
1/3	Keith Bottomley	N/A	<i>What are your favourite buildings and spaces along the river?</i> People mentioned spaces both north and south of the river. They included Custom House and Billingsgate, Tower Bridge, London Bridge, Butlers Wharf/Shad Thames, Tower of London, Tate Modern frontage. Clink Street. The first floor public viewing area to east of St Magnus the Martyr and Fishmongers' Hall. The view from the river of St. Pauls was also mentioned.	Each of these buildings is mentioned in the Thames Strategy <ul style="list-style-type: none"> <li>• Custom House &amp; Billingsgate (para 7.23.1)</li> <li>• Tower Bridge &amp; London Bridge (para 7.39)</li> <li>• Public viewing area east of St Magnus the Martyr (para 7.27.1) – <b>this paragraph has been amended to include reference to the public viewing area.</b></li> <li>• Fishmongers Hall (para 7.18.1)</li> <li>• View of St Paul's from the river (para 4.5.1)</li> </ul> Butler's Wharf/ Shad Thames, Tate Modern frontage and Clink Street are in the London Borough of Southwark therefore it would be inappropriate to include them in this strategy.
1/4	Keith Bottomley	N/A	<i>What important features should be incorporated into any new developments? For example, shared uses and public facilities, new pedestrian routes through large schemes, interesting design features and use of materials etc.</i> Any new developments should <b>not interfere with the objective of opening up the river walkway.</b> There is a unique opportunity to incorporate some	Improvement in pedestrian routes and completion of the riverside walk are emphasised in several places within the Thames Strategy. The encouragement for office led commercial development is intended to include provision of cafés and restaurants where appropriate. (para 2.4.1) Paragraph 3.3.1 emphasises the need to protect the river for river

			attractive <b>cafes/restaurants</b> which will allow people to enjoy the views of the river. There should be <b>no loss of river space to non-river uses</b> . The introduction of <b>planted areas/pocket parks</b> would be welcome. <b>Avoid shops and cafes bars etc. other than in ground floor of buildings</b> . Additional <b>toilets</b> should be provided. Public routes through <b>and additional public spaces and green areas</b> would be a good idea. New <b>pedestrian routes</b> , particularly west of London Bridge would help.	related uses <b>Para 5.1.1 has been amended by adding the following sentence: <u>Public toilets are provided in two locations on the riverside and participation in the Community Toilet Scheme adds to provision</u></b>
1/5	Keith Bottomley	N/A	<i>How do you use the open spaces now, and what do you feel is lacking in the open spaces that could improve use in the future? For example, new amenities, such as additional seating, or space for new pursuits</i> The main <b>aim should be the achievement of a continuous riverside walkway</b> . The introduction of planted areas/pocket parks is also desirable. More and better <b>seating</b> to allow for relaxing and taking in the ambiance will add to enjoyment. Also some <b>open buildings</b> worth visiting.	The role of the Riverside Walk Enhancement Strategy in identifying potential for planted areas, pocket parks and additional seating is recognised in the Thames Strategy. The majority of buildings along the riverside in the City are in private ownership. Greater public access to them could bring benefits by making the area more permeable but would need owner support. <b>No change needed</b>
1/6	Keith Bottomley	N/A	<i>What type of businesses and/or shops do you feel are lacking and would like to see more of along the river? For example, which independent retailers do you support and value? Do you feel there would be local demand for small office or workshop spaces, etc.?</i> The river should not become a shopping street. There would always be <b>demand for small office or workshop space on the river</b> at affordable rent, but good location is invariably available only at a premium for housing and business and City riverside land is surely a hugely valuable and hence expensive commodity. How can <b>space be made affordable for small businesses</b> who would not be able to afford to rent spaces along the river which are likely to fetch premium rates and also the City tends to be a bit empty in the evenings at certain times of the year. Retail over shops; workshops with crafts over offices; offices overhead are seen as attractive options.	The encouragement for office led commercial development could include workshops and retail as long as they do not conflict with residential amenity or neighbouring uses. <b>No change needed</b>
1/7	Keith Bottomley	N/A	<i>What would make the streets more attractive and user-friendly? For example, shared use of streets by both vehicles and pedestrians, better crossings etc.?</i> The city is famous for its passageways, alleys and pedestrian streets. Creation of spaces of this kind, away from areas of heavy through traffic should be the overriding aim. Active frontages are needed at ground level with work spaces of all kinds as necessary above. <b>Better signage</b> is required to direct people to and from the river and other sights and places of interest. Consideration should be given to use <b>of Lower Thames Street as a cycle and pedestrian area over cars</b> , and less intimidating and easier / quicker crossing of Lower Thames Street.	The Riverside Walk Enhancement Strategy identifies areas along the riverside where signage should be enhanced. The Mayor is implementing proposals for an east west Cycle Superhighway along Thames Street. (Para 7.4) <b>Para 7.4 has been updated to reflect implementation of cycle superhighway</b>
1/8	Keith Bottomley	N/A	<i>How can access to the river be improved?</i> Creation of a <b>continuous walkway but not on or at the expense of river space should be the aim</b> . <b>Clearly marked and organised routes</b> from all Underground Stations. More map boards, especially at Monument. Part of the walkway in front of customs house should be opened up as it is	The Riverside Walk Enhancement Strategy promotes improved signage and enhanced public access at Custom House. <b>No change needed</b>

1/9	Keith Bottomley	N/A	quite restrictive. <i>Would you like to see Swan Lane Pier re-opened?</i> The prevailing view is <b>yes</b> . Use will depend on which services will be accessed from it. It would allow use of the <b>water transport to get to Canary Wharf</b> without having to walk across London bridge or to the Tower which is often quite congested. Water taxis would be a great addition.	Reinstatement of Swan Lane Pier is an aspiration included in the Thames Strategy (para 5.6.1, 7.20.4 and 7.22.1) <b>No change needed</b>
1/10	Keith Bottomley	N/A	<i>Are there any other themes that the strategy should include? These might be related to the qualities and places you think make the riverside special.</i> Incorporate a variety of places, small piazzas and other pedestrian spaces. <b>No vehicles or cycles on river frontage</b> . More <b>soundscapes</b> , which are particularly appreciated by blind and partially sighted would be very welcome. The front of Customs House should be made wider.	Paragraph 5.1.1 emphasises that cycling and skateboarding are not permitted on the riverside walk. Paragraph 5.4.1 promotes the use of soundscapes as an element of inclusive design. <b>No change needed</b>
1/11	Keith Bottomley	N/A	<i>Are there any other comments that you'd like to make regarding the river more generally, outside those directly related to Bridge Ward?</i> The Thames path along the <b>south bank</b> is surely London's <b>most popular and most used urban "park"</b> . It is a great and safe way to enjoy the City and Westminster. The north bank offers great opportunities to <b>match it, initially between Blackfriars Bridge and the Tower of London</b> , at least.	It is important to strike a balance between vibrancy and quiet spaces on the City's riverside, recognising the role it plays in supporting the commercial City, providing visitor routes between attractions and maintaining amenity for riverside residents and residential uses. <b>No change needed</b>
2/12	David Coleman	N/A	I am responding to your recent invitation for comments.  I was Regional Director of the Countryside Commission when the Thames Path National Trail was designated and I should like to comment on the treatment of public access to the riverside.  The <b>City has an outstanding record</b> in rising to the challenge of forging new riverside access in such a challenging environment. The vast <b>increase in public use of the riverside over the past 30 years</b> has clearly demonstrated the public and tourism benefits which flow.	Comment noted <b>No change needed</b>
2/13	David Coleman	N/A	Section 5.3 should be worded much more forcefully, making it clear that <b>completion of a continuous riverside path is a high priority</b> and setting a stretching target date. If redevelopment opportunities are likely to be delayed, construction of a temporary cantilevered walkway should be considered. The weak language of "longer term aim" (7.14); "wherever possible" (7.19); "access ... will be sought" (7.25.2) does not reflect the scale of public benefits which this measure will deliver.	<b>Change the wording as follows:</b> <b>para 7.14 "....Completion of the Riverside Walk at Queenhithe is a longer term aim- Planning permission has been granted..."</b> <b>para 7.19 "Enhancing riverside open space is a key ambition with inclusive public access wherever it is possible, bearing in mind that the stretch of riverside walk from Queenhithe to Cousin Lane is rather narrow"</b> <b>para 7.25.2 "Greater public access and more inclusive use of these spaces will be sought would be expected to complement any redevelopment."</b>
2/14	David Coleman	N/A	Your Open Spaces Strategy notes that the riverside is one of only three areas in the City which meets the World Health Organisation guidance for noise levels in gardens and recreation areas. Peace and quiet is, for obvious reasons, a very rare commodity in the City and one much valued by workers, residents and visitors.	It is important to strike a balance between vibrancy and quiet spaces on the City's riverside, recognising the role it plays in supporting the commercial City, providing visitor routes between attractions and maintaining amenity for riverside residents and educational uses. The Thames Strategy provides this balance emphasising the need to protect residential amenity and avoid undesirable impacts on the City

			<p>It should not be City policy to "enliven" the riverside (5.2.3; 5.3.2; 7.5.2; 7.5.3; 7.20.1; 7.27.1). Quite the reverse. The South bank is exceedingly lively; the USP of the City stretch of the North bank is peace and quiet. SPD should establish this as the guiding principle, making it clear that planning permission will not be granted for uses likely to erode the peace and quiet of this resource.</p> <p>This change will also help meet your concerns about residential amenity (7.14; 7.15.1; 7.30.2) and impacts on City of London School for Boys (5.2.2; 7.9).</p>	<p>of London School for Boys.</p> <p><b>No change needed</b></p>
3/15	Michael V Cooper	N/A	<p>St Magnus House at 3 Lower Thames Street is <b>not a 1960s building</b>, but was constructed between 1977 and 1980 following the closure and demolition of New Fresh Wharf.</p> <p><a href="https://en.wikipedia.org/wiki/Fresh_Wharf#/media/File:New_Fresh_Wharf_near_London_Bridge.jpg">https://en.wikipedia.org/wiki/Fresh_Wharf#/media/File:New_Fresh_Wharf_near_London_Bridge.jpg</a></p>	<p><b>Text amended to state that both buildings were completed in the 1980s</b></p>
3/16	Michael V Cooper	N/A	<p>The draft planning document states that "The primary planning aim for this stretch of the riverside is that it should provide office led commercial development with associated uses which serve the needs of the business City. This could include retail and conference or event space which would not be compatible with residential development." I would like to suggest that a <b>narrow focus on office development may not be commercially viable</b> and overlooks more creative uses for the area.</p> <p>The riverside between St Magnus the Martyr church and old Billingsgate Market has only been used as office space since the 1980s. Both buildings are unloved. Northern &amp; Shell (with whom I have no connection) has already convincingly explained why an office redevelopment on this site would be commercially unviable. There seems to be no good reason why <b>other uses, including residential, hotel and tourism as well as retail</b> should not be allowed. This could recreate the multi-purpose usage and feeling of community that existed in the area until the 1970s. New Fresh Wharf and Billingsgate Market are history, along with the businesses (such as Joe's Number One Snack Bar at 1 Lower Thames Street) that served them. However, it is surely time to start to reverse the disastrous planning decisions of the 1960s and 1970s, included the widening of Lower Thames Street and the demolition of the Coal Exchange, that has made the area so sterile.</p>	<p>The Local Plan policy CS 9 and Thames Strategy SPD both emphasise Office led commercial development. This does not preclude supporting uses such as hotels, conference and event space and retail which would enliven the area without compromising residential amenity in the existing residential cluster at Queenhithe.</p> <p>The issue of land use along the riverside was considered at the Local Plan examination in October 2014. The Inspector's Report stated that "Policy CS9, which gives more emphasis to office-led commercial development along the Riverside, than was the case in the Core Strategy, is sound in view of the likely pressures for residential development. I am satisfied that the site specific viability considerations are able to be given due weight, and that the City Corporation has already demonstrated sufficient flexibility in that process. Therefore I do not support any different approach for the Thames Riverside."</p> <p>Since this Local Plan policy approach was found to be sound at examination, it would be inappropriate to alter it through a Supplementary Planning Document.</p> <p><b>No change needed</b></p>
3/17	Michael V Cooper	N/A	<p>Until the Second World War Fresh Wharf was used not only for cargo, but also for passenger boats. The entrance to Fresh Wharf advertised daily sailings and establishments such as the Adelaide Hotel (on the site of Adelaide House) and the Steam Packet Inn on Lower Thames Street catered for tourists and day trippers travelling by boat. <b>There may be scope for a new pier</b> at Fresh Wharf and a new hotel. In any event <b>there is scope to combine:-</b></p>	<p>The Thames Strategy promotes the reinstatement of Swan Lane Pier and does not preclude the development of additional piers at other locations.</p> <p>The Riverside Walk Enhancement Strategy and the Thames Strategy demonstrate a strategic approach which includes improvement of access between the river and Lower Thames Street (para 5.5.1)</p> <p><b>Paragraph 5.5.1 has been amended to add details of the mechanism for improved access across Thames Street. 5.5.1 The City of London</b></p>



			<p>a) the <b>riverside walk enhancement strategy</b>,</p> <p>b) better North/South <b>access between the river and Lower Thames Street</b>,</p> <p>c) further improvements to allow <b>pedestrians to cross Lower Thames Street</b>, whether via crossings, footbridges or underpasses,</p> <p>d) the <b>East-West Cycle Superhighway</b> between Lower Thames Street and Tower Hill,</p>	<p><b><u>Corporation will continue to work with Transport for London, the Highway Authority for Upper and Lower Thames Street, on further improvements to existing pedestrian crossings and the introduction of new crossing facilities to further improve connectivity.....</u></b></p> <p>Para 7.4 identifies the East West and the North South Cycle Superhighways which will present challenges particularly at the Temple Place to Blackfriars Bridge.</p>
3/18	Michael V Cooper	N/A	<p>e) a cultural strategy, taking account of the fact that from Fishmongers' Hall and London Bridge to Tower Bridge and the Tower of London there is a rich diversity of cultural assets, including the Monument, Billingsgate Roman Bath House, St Magnus the Martyr church, old Billingsgate Market, the Custom House and the River Thames.</p> <p>Surviving monuments could be linked to the wealth of archaeological information that has been gleaned over recent decades and to historical resources, e.g. in respect of Old London Bridge and the Great Fire of London, to <b>create a vibrant cultural quarter for the benefit of tourists, residents and City workers</b>.</p> <p>I would therefore urge the City Corporation to revisit its planning assumptions and <b>think much more creatively</b> about the potential uses for this stretch of the riverside.</p>	<p>Paragraph 7.23.1 highlights the wealth of heritage assets particularly in the London Bridge to Water Lane section of the riverside and paragraph 7.24 emphasises the need to identify suitable future uses for historic buildings in the area, which conserve and enhance their historic significance and commercial heritage.</p> <p><b>Paragraph 7.4 has been amended to include reference to the Riverside Walk Enhancement Strategy which promotes improvements to make the area more attractive for tourists, residents and City workers.</b></p>
4/19	Peter Massini	All London Green Grid	<p>Government guidance indicates that: 'Local Nature Partnerships are not subject to the requirements of the duty. But local planning authorities and the public bodies that are subject to the duty must cooperate with Local Enterprise Partnerships and Local Nature Partnerships and have regard to their activities when they are preparing their Local Plans'.</p> <p>The All London Green Grid partnership is recognised by Defra as the LNP for London. However, like many other LNPs, the All London Green Grid partnership does not have a governance structure that allows it to speak with a single voice on matters such as statutory consultations.</p> <p>Nevertheless, <b>provided the Thames Strategy is in conformity with London Plan policies</b> with regard to policies on green infrastructure, biodiversity, trees &amp; woodland, and the Blue Ribbon Network <b>your authority will be having regard to the activities and objectives of the London LNP</b>.</p>	<p>This strategy is in line with the City of London Local Plan, which is in conformity with the London Plan.</p> <p><b>No change needed</b></p>
5/20	John Schofield	City of London Archaeological Trust	<p><b>p30 para 30</b>. You have confused the two Baynard Castles. The first, established 1087, was to the north, on the west side of St Andrew's Hill. But this is outside the area being discussed, so <b>rephrase this para</b> as:</p>	<p><b>Paragraph 7.8.3 altered to read:</b>  <b>Baynard's Castle was originally built in 1087 on the west side of St Andrew's Hill, and destroyed to build the Blackfriars priory after 1275. A</b></p>

			Baynard's Castle was originally built in 1087 on the west side of St Andrew's Hill, and destroyed to build the Blackfriars priory after 1275. A separate castle-like house called Baynard's Castle was built on the waterfront after about 1470. The foundations.. [continue]	<b><u>separate castle-like house called Baynard's Castle was built on the waterfront after about 1470. The foundations..</u></b> Baynard Castle scheduled ancient monument was built in 1087 and destroyed to build Blackfriars Priory in 1275
5/21	John Schofield	City of London Archaeological Trust	p32 para 7.13.3 put the sentence about the Scheduled Ancient Monument at the end of the previous paragraph, where it belongs.	<b>Text moved as suggested</b>
5/22	John Schofield	City of London Archaeological Trust	p37 para 7.27.1. The W of the two buildings between St Magnus and Billingsgate market is of 1978, not the 1960s.	This building was completed in 1980 <b>Text amended to state that both buildings were completed in the 1980s</b>
5/23	John Schofield	City of London Archaeological Trust	p38 para 7.28.2. Not correct. All Hallows is of Anglo-Saxon date, as witnessed by the arch in its tower which survives, and although badly bombed in World War II, is medieval in outline. Its tower is of 1658.	<b>Paragraph 7.28.2 altered to read: All Hallows Church (Grade 1 listed) is of Anglo Saxon date, as witnessed by the arch in its tower which survives and although badly bombed in World War II, is medieval in outline. Its tower is of 1658. was established in 675AD and rebuilt several times. The current church dates from 1658 and is Grade I listed.</b>
6/24	Simon Vince	Heathrow Airport Ltd	** this response has been summarised** The three draft Supplementary Planning Documents for the City of London have been examined from an aerodrome safeguarding perspective and although we have no safeguarding concerns we would like to add the following comments. Aerodrome Safeguarding Aerodromes important to the national air transport system are officially safeguarded by the Civil Aviation Authority and the process of ensuring that their operation and development is not inhibited is an integral part of the town planning system. This should take account of building height & structure, potential for electronic interference, lighting, bird strikes. Developers should engage early with the Planning Authority and the airport operator and consult the safeguarding map. Mitigation measures should be used wherever possible. The Planning Authority should consult the airport operator Civil Aviation Authority (CAA), NATS En Route Ltd. (NERL) and the Ministry of Defence (MOD) on any wind turbine development within 30 miles of Heathrow	Comment noted <b>No change needed</b>
7/25	Katharine Fletcher	Historic England	Thank you for the opportunity to comment on the Thames Strategy SPD setting out the policies and strategy for the Thames Policy Area in the City of London. As the Government's statutory adviser on the historic environment Historic England is keen to ensure that the protection and enhancement of the historic environment is fully taken into account at all stages and levels of the planning process. In the context of this consultation we should mention that Historic England has recently been formed from a division of the responsibilities of English Heritage. From 1 April 2015 Historic England takes forward the role and responsibilities for providing historic advice previously provided by English	Comment noted <b>No change needed</b>

			<p>Heritage. The name of English Heritage is retained for the new charity caring for the National Collection of historic properties. As Historic England, we continue to protect and champion England's wider historic environment. This includes listing, planning, grants, research, advice and public information.</p> <p><b>The Thames Policy Area contains heritage assets of great significance.</b> In addition to listed buildings, conservation areas and registered historic parks and gardens, the archaeology of the area is nationally important, and comprises both scheduled monuments and undesignated archaeological remains. The Thames foreshore is London's most extensive archaeological site. The contribution of the Thames riverside area in the City to the setting of the Tower of London World Heritage Site (WHS) just outside the policy area is also a key consideration.</p> <p><b>We welcome the recognition in the document of the long history of this area,</b> and the influence of this on the present townscape and cultural interest above and below ground. The heritage assets are a defining quality of the riverside and represent a positive resource both for their intrinsic historic interest and also for their contribution to economic and social values. Conserving and enhancing the riverside heritage is sustainable in every sense.</p>	
7/26	Katharine Fletcher	Historic England	<p>While we support the high profile given to the significance of heritage in the City's Thames Strategy document there are some ways we suggest that this should be strengthened. These are as follows:</p> <p>1. The objectives for the strategy are set out in <b>para 1.5</b>. Within these there is a need to reconcile the demands made on the Thames, in terms of increased river transport (objective 2), with the related impacts this may have, including on the conservation of heritage assets (objective 3). The heritage assets of the Thames foreshore are highly vulnerable to erosion. It would therefore be helpful to <b>include a section on balancing and reconciling potentially conflicting interests</b>. This would align the SPD with the approach in paras 8 and 152 of the National Planning Policy Framework (NPPF).</p>	<p><b>The following sentence has been added to paragraph 1.5 :</b>  <b><u>Balancing and reconciling potentially conflicting interests arising from these objectives is an important intention of this strategy.</u></b></p>
7/27	Katharine Fletcher	Historic England	<p>2. The Future Issues identified in <b>section 3.3 should include a bullet referring to the opportunities for conservation of heritage assets, and the potential vulnerabilities of this resource</b>. The document identifies some of the key issues and challenges relating to the historic environment in section 7.</p>	<p><b>Additional Bullet Point added at 3.3</b>  <b><u>• Opportunities for conservation of heritage assets, ensuring that new development does not detract from these assets and their settings</u></b></p>
7/28	Katharine Fletcher	Historic England	<p>3. Since the historic environment is a continuum the boundary of the SPD area is, in this sense, an artificial one. Figure 2 highlights this problem – the full extent of scheduled monuments that span the boundary are shown, helpfully, where they lie outside the area; however, where there are scheduled monuments on the border to the north they are not shown. We <b>suggest that Figure 2 identifies all heritage designations immediately bordering the area, including the World Heritage Site</b>. It would be suitable to emphasise that, in line with the historical description in the SPD, the riverside area has a strong relationship to its hinterland. In this respect the</p>	<p>Figure 2 shows heritage assets in the Thames Policy area. Where an asset is partially within the Thames Policy Area it has been shown. Showing assets that are wholly outside the area or indeed outside the City would be confusing. The Thames Foreshore is not a specifically designated Heritage Asset – it is an area with archaeological potential in common with the whole of the City. Therefore it would be inappropriate to mark it on the map</p> <p><b>Paragraph 2.4.3 has been amended with the following additional sentence:</b></p>

			section detailing policy from adjoining boroughs is useful, and it is a matter of drawing the point out more fully. We further recommend that the archaeological significance of the Thames foreshore is identified in Figure 2, and explained in section 4.	<b><u>The Thames foreshore is of particular archaeological significance.</u></b>
7/29	Katharine Fletcher	Historic England	4. The Tower of London World Heritage Site is rightly given prominence in the document within section 4. Its setting should be understood in terms of the wide definition in the NPPF ie. it encompasses 'the surroundings in which a heritage asset is experienced' . As the extent is not 'fixed' it should be made clear in the document that consideration of setting applies to a wider area than the 'locally defined setting' or the areas lying within the Mayor's LVMF views. While this point requires particular clarification in relation to the Tower of London it also has relevance in respect of the Monument, and other heritage assets in the area.	The Local Plan Policy DM12.1 requires developers to take account of the impact of development on the setting of heritage assets. There is no need to repeat this guidance in the SPD. The City of London Corporation will apply the Protected Views Policy CS13 and the City of London Protected Views SPD. In addition to this Historic England will have the opportunity to highlight any issues related to the surroundings in which a heritage asset is experienced through their role as statutory consultee on planning applications. <b>Paragraph 4.6.1 has been amended to include reference to the Protected Views SPD as follows:</b> <b>Where necessary developers should provide evidence with the application of how their development proposal responds to the protected views requirements in line with the City of London Protected Views Supplementary Planning Document</b>
7/30	Katharine Fletcher	Historic England	5. Historic England supports inclusive design in relation to access to the Riverside Walk, as outlined in section 5.4. Our guidance on this matter is available on our website at: <a href="https://www.historicengland.org.uk/advice/technical-advice/easy-access-to-historic-buildings-and-landscapes/">https://www.historicengland.org.uk/advice/technical-advice/easy-access-to-historic-buildings-and-landscapes/</a> . We would be pleased to discuss proposals where sensitive adaptation of heritage assets is required.	Support noted <b>No change needed</b>
8/31	Heather Hilburn	Historic Royal Palaces	We are pleased to see that the Draft Thames Strategy supports our vision of enhancing the experience of Tower visitors and protecting the Tower for future generations. In particular, we are pleased to see references to the Tower of London Local Setting Study and World Heritage Site Management Plan, as well as the protection of the Tower's Outstanding Universal Value.	Support noted <b>No change needed</b>
8/32	Heather Hilburn	Historic Royal Palaces	We are also pleased to see the detailed consideration of the character, issues and opportunities in the 'Water Lane to All Hallows by the Tower' and 'Mark Lane to Mansell Street' areas which form part of the Tower's Local Setting. We offer the following suggestions to build on the points you have made. <b>Water Lane to All Hallows by the Tower</b> As you have seen from our Tower Future Thinking Plan, we have been reviewing how we can better accommodate rising visitor numbers, in order to reduce the length of entry queues. To accomplish this objective, we anticipate a future need to extend the Thames Path south of Wharfinger Cottage. This would have the additional benefits of removing the present severance to the Thames Path and providing additional spectator space for river activities. Whilst Wharfinger Cottage is located in Tower Hamlets, the west side of a Thames Path extension would likely reach the City.	<b>The following sentence has been included in the first bullet point of paragraph 7.29.</b> <b><u>There is potential for improved integration of the Thames Path in the City with that in Tower Hamlets.</u></b>

			We would like Thames Strategy to note the present severance in the Thames Path next to Wharfinger Cottage as an issue (paragraph 7.29) and note a potential opportunity to extend the Thames Path to overcome this (paragraph 7.32).	
8/33	Heather Hilburn	Historic Royal Palaces	<p><b>Mark Lane to Mansell Street</b></p> <p>In future, we wish to welcome our visitors outside Tower Hill Underground Station, and provide them with information about the Tower and the surrounding area. We also want to make it easier for visitors to find the public execution site in Trinity Square Gardens. We would like to see the following text added at the end of paragraph 7.34.3</p> <p><i>It will be important to provide and maintain suitable public realm and wayfinding within and adjoining the hotel cluster, accommodating access to the hotels and Tower Hill Underground Station and amenity space for visitors to the Tower of London.</i></p> <p>This would echo a point made in the report in paragraph 7.29 in relation to the Water Lane to All Hallows by the Tower area.</p> <p>We believe that the Draft Thames Strategy represents a significant step forward and will be a valuable tool for future development. Please let me know if you wish to discuss our comments in further detail.</p>	<p>The following sentence has been added to paragraph 7.34.3:</p> <p><b><u>It will be important to provide and maintain suitable public realm and way-finding within and adjoining the hotel cluster, accommodating access to the hotels and Tower Hill Underground Station and amenity space for visitors to the Tower of London.</u></b></p>
9/34	Hong Chen	London Borough of Tower Hamlets	<p>Thank you for inviting the London Borough of Tower Hamlets Council (LBTH) to participate in the public consultation on the draft Thames Strategy undertaken between 18th March and 30th April 2015.</p> <p>Tower Hamlets Council is keen to cooperate with the City of London Corporation and other relevant stakeholders to ensure that the Thames Strategy represents an approach that is in line with policies and strategies and provides a framework which benefits the wider Thames environment. In particular, we would like to work closely with you to address some key challenges identified in the consultation document (page 12) together. Our comments to the consultation document are set out as follows for your consideration.</p> <ul style="list-style-type: none"> <li>Paragraph 4.2.3 states that development in the Thames and Riverside could affect some of the views and any potential impact should be assessed through Townscape and Heritage Impact Assessment. Development and its impact on the Tower Conservation Area (and other conservations areas and heritage assets) must be assessed as part of the Environmental Statement- Townscape and Heritage Impact Assessment. LBTH would welcome early consultation on proposals which might have an impact on the character of the Tower Conservation Area.</li> </ul>	<p>The City of London Corporation will apply the Protected Views Policy CS13 and the City of London Protected Views SPD. In addition to this the London Borough of Tower Hamlets will have the opportunity to highlight any issues related to the Tower Conservation Area through their role as statutory consultee on planning applications.</p> <p><b>Paragraph 4.5.1 has been amended to include reference to the Protected Views SPD as follows:</b></p> <p><b>Where necessary developers should provide evidence with the application of how their development proposal responds to the protected views requirements in line with the City of London Protected Views Supplementary Planning Document</b></p>

			The most significant threat to the Conservation Area is the pressure for buildings of excessive height and bulk beyond its boundaries to the detriment of its character and appearance. In making any planning decision the impact upon the character and appearance of the Conservation Area must be considered. In the case of the Tower the setting and backdrop are critical to its character and must be protected and the City of London forms part of this setting and backdrop.	
9/35	Hong Chen	London Borough of Tower Hamlets	<ul style="list-style-type: none"> <li>Paragraph 5.1 Recreation – It is noted that cycling and skateboarding are not allowed on the riverside walk in the City. Our spatial policy (SP04) of the Core Strategy (2010) encourages to improve accessibility to and along waterspaces to maximise usability and promote these places for cultural, recreational and leisure activities. Cycling is allowed on the riverside walk in Tower Hamlets.</li> </ul>	<p>The riverside walk in the City is very narrow in places with restricted turns where conflicts between cyclists and pedestrians could be dangerous. It would be inappropriate to allow cycling and skateboarding on the City's riverside. The Mayor's east west cycle route will provide a segregated cycle route parallel to the Thames path along Thames Street.</p> <p><b>No change needed</b></p>
9/36	Hong Chen	London Borough of Tower Hamlets	<ul style="list-style-type: none"> <li>Paragraph 5.2.4 Tower Millennium Pier – LBTH strongly supports to further explore the opportunities to connect the pier with the Riverside Walkway through public realm improvements at Three Quays and Sugar Quay. In addition, it is suggested consideration should also be given to improve the pier facilities due to its popularity with tourists.</li> </ul>	<p>Support noted</p> <p>Improved pier facilities would be located in the London Borough of Tower Hamlets. Their impact on the City's Riverside Walk would be considered through the normal consultation routes.</p> <p><b>No change needed</b></p>
9/37	Hong Chen	London Borough of Tower Hamlets	<ul style="list-style-type: none"> <li>Paragraph 7.35.3 – Since the adoption of the Tower Hamlets Core Strategy in 2010, Aldgate (which is very close to Mansell street) has been gradually transformed with a mixed office and residential. This has been reflected in the emerging City Fringe/Tech City Opportunity Area Planning Framework, which states: "The pace of residential development in and around Aldgate means that the area is likely to be more residential in nature than envisaged when the last detailed master planning exercise took place." Tower Hamlets Council will take the changing nature of Aldgate into consideration in the process of reviewing its Local Plan. In order to resolve potential cross boundary policy conflicts, paragraph 7.35.3 should be amended as follows: <ul style="list-style-type: none"> <li>"The City Corporation will continue to promote office-led commercial development on the riverside to support the business city, while taking account for mixed use development coming forward in Aldgate in line with the London Plan and Tower Hamlets' Local Plan". This is to ensure neighbouring boroughs to work collaboratively and effectively to resolve conflict development pressures and strike an appropriate balance in different uses and contribute positively to the river related uses."</li> </ul> </li> </ul>	<p>Aldgate is outside the Thames Policy Area and is some distance from the River Thames. The City of London Local Plan Policy CS8 identifies Aldgate as an office and residential location and commits the City to identifying and meeting residents' needs.</p> <p><b>No change needed</b></p>
9/38	Hong Chen	London Borough of Tower Hamlets	Tower Hamlets Council supports the overall approach set out in the consultation document. We would welcome the opportunity be more closely involved in any discussions around the above points if required.	<p>Comment noted</p> <p><b>No change needed</b></p>
10/39	Angela	Marine	** this response has been summarised**	Comment noted

	Gemmill	Management Organisation	<p>Thank you for inviting the Marine Management Organisation (MMO) to comment on the above consultation. The MMO has reviewed the document and whilst we have <b>no specific comments</b> to make we would like to draw your attention to the remit of our organisation as you may wish to be aware of this in relation to the consultation.</p> <p>The MMO is responsible for preparing marine plans for English inshore and offshore waters. In our duty to take all reasonable steps to ensure compatibility with existing development plans.</p> <p>The East Inshore and East Offshore marine plans were published in April 2014. Until such time as a marine plan is in place for the South East plan area we advise local authorities to refer to the Marine Policy</p> <p>The MMO's guide for Local Authority planners explains what local council planners need to know about marine planning (<a href="https://www.gov.uk/government/publications/marine-planning-a-guide-for-local-authority-planners">https://www.gov.uk/government/publications/marine-planning-a-guide-for-local-authority-planners</a>).</p>	<b>No change needed</b>
11/40	Gillian Fensome	Natural England	<p>Natural England is a non-departmental public body. Our statutory purpose is to ensure that the natural environment is conserved, enhanced and managed for the benefit of present and future generations, thereby contributing to sustainable development.</p> <p>We have considered the contents of the documents submitted to us and confirm that we have the following comments to make:</p> <p>Riverside Walkway</p> <p>We note that opportunities to improve or enhance the Riverside Walkway are mentioned several times in the Thames Strategy SPD. We confirm that, if it is The <b>Thames Path</b> you are referring to, which is a nationally designated long distance walking route, <b>we are in support of carrying out improvements or enhancements to it and to realigning it back to alongside the Thames.</b></p>	<p>Support noted</p> <p><b>No change needed</b></p>
11/41	Gillian Fensome	Natural England	<p>Biodiversity</p> <p><b>We are in support of opportunities to improve biodiversity in the SPD area, through the use of Green Infrastructure (GI) and Sustainable Urban Drainage Systems (SuDS) (mentioned at page 24).</b></p> <p>The National Planning Policy Framework (NPPF) states at paragraph 114 that:</p> <p>"Local planning authorities should ... set out a strategic approach in their Local Plans, planning positively for the creation, protection, enhancement and management of networks of biodiversity and GI".</p>	<p>Support noted</p> <p><b>No change needed</b></p>
11/42	Gillian Fensome	Natural England	<p>Strategic Environmental Assessment (SEA) screening</p> <p>We agree that the Thames Strategy SPD is unlikely to have significant environmental effects and therefore that a full Sustainability Appraisal/SEA of it is not required.</p>	<p>SEA Screening response noted</p> <p><b>Included in SEA Screening Report</b></p>



12/43	DP9	Northern & Shell	Our client is owner and occupier of 10 Lower Thames Street, which is the blue glass clad building located between 3 Lower Thames Street and Billingsgate Market. The site falls within the area identified in between London Bridge to Water Lane in the draft Thames Strategy SPD. We would firstly question why the draft Thames Strategy SPD has chosen to geographically separate the area between London Bridge and All Hallows. In our view, the location between London Bridge and All Hallows forms a cohesive geographical area which is characterised by a mix of office, leisure/conference, retail and residential uses. It is an area which is distinctly different in character than that to the west of London Bridge, by not being dominated by office uses. We would recommend that these two areas are considered together under the Strategy	The predominant land use between London Bridge and Water Lane is offices with Old Billingsgate Market and St Magnus the Martyr church being the main non office buildings along this stretch of riverside. By contrast the section from Water Lane to All Hallows by the Tower includes a mix of uses including hotel and residential, offices and place of worship. The issues for this area relate largely to its setting adjacent to the Tower of London World Heritage Site therefore it is logical to separate these two areas. <b>No change needed</b>
12/44	DP9	Northern & Shell	We are supportive of the overall draft Strategy's objectives of enhancing the riverside walkway in terms of the provision of higher quality public realm and activation. However as we previously contended in our representation to the Local Plan and the Riverside Enhancement Strategy, to achieve these objectives will require our client's site to be developed for a mixed use office and residential development as redevelopment for offices alone is not viable. Therefore the statement at 7.27.1 that "The intention for this site is that it should be retained as office led commercial development realising the opportunity to enliven the riverside and provide pedestrian access and views of the river" will not be realised. In any event, we consider that it is not appropriate for the Thames Strategy as an SPD to set out what land uses would be appropriate on the site. An SPD should not deal with land use matters.	The issue of land use along the riverside was considered at the Local Plan examination in October 2014. The Inspector's Report stated that "Policy CS9, which gives more emphasis to office-led commercial development along the Riverside, than was the case in the Core Strategy, is sound in view of the likely pressures for residential development. I am satisfied that the site specific viability considerations are able to be given due weight, and that the City Corporation has already demonstrated sufficient flexibility in that process. Therefore I do not support any different approach for the Thames Riverside." Since this Local Plan policy approach was found to be sound at examination, it would be inappropriate to alter it through a Supplementary Planning Document. <b>No change needed</b>
12/45	DP9	Northern & Shell	We also note that the "site" referred to in 7.27.1 relates to both 3 and 10 Lower Thames Street. As these sites are in different ownership, there can be no guarantee that they will be brought forward together as one single development. We consider that the text should therefore be amended to make a distinction between each site.	<b>Paragraph 7.27.1 has been changed to read:</b> <b>The site area between the Church of St Magnus the Martyr and Old Billingsgate Market is currently in office use comprising two separate buildings...</b> <b>The remainder of the paragraph has been amended to refer to areas and buildings rather than a single site.</b>
12/46	DP9	Northern & Shell	Our client considers that the approach to land use needs to work alongside the public realm improvement. There is a need to provide a mix of uses along the riverside which generate greater levels of activity during the day and evening. At ground floor, this will require the introduction of cafes/restaurants but also the introduction of residential uses so that the area is not dominated by office uses alone. The redevelopment of 10 Lower Thames Street for a mix of office/residential and ground floor active uses could deliver an active more vibrant riverside in this area and realise objectives of the draft Thames Strategy. Without the introduction of a mix of uses along this part of the riverside, it is considered that the public realm will remain sterile after office hours, irrespective of the improvement in quality. In the context of the above we object to a number of references within the	The issue of land use along the riverside was considered at the Local Plan examination in October 2014. The Inspector's Report stated that "Policy CS9, which gives more emphasis to office-led commercial development along the Riverside, than was the case in the Core Strategy, is sound in view of the likely pressures for residential development. I am satisfied that the site specific viability considerations are able to be given due weight, and that the City Corporation has already demonstrated sufficient flexibility in that process. Therefore I do not support any different approach for the Thames Riverside." Since this Local Plan policy approach was found to be sound at examination, it would be inappropriate to alter it through a Supplementary Planning Document.



			<p>London Bridge to Water Lane section of the draft SPD. The first bullet point states that:</p> <p><i>"This part of the riverside offers the potential to enhance the City's attractiveness as a business location through the retention of office led commercial development, utilising prime riverside office sites with iconic views of Tower Bridge and the River Thames. These uses would not be compatible with residential development therefore residential development along this part of the river will be discouraged"</i></p> <p>We object to the statement that office uses are not compatible with residential development. London's success is based on its mix of uses and the juxtaposition of residential and office uses which create a vibrant and active City. The riverside location in particular is one area where office uses alongside residential can assist in ensuring day time and night time activity enliven public spaces. We would question again whether it is appropriate for the SPD to be dealing with land use issues. We would <b>recommend that</b> the wording of the first bullet point of 7.24 is amended as follow:</p> <p><i>"This part of the riverside offers the potential to enhance the City's attractiveness as a business location through the retention of office led commercial development, utilising prime riverside office sites with iconic views of Tower Bridge and the River Thames. These uses would not be compatible with residential development therefore residential development along this part of the river will be discouraged"</i></p>	<p><b>Paragraph 7.24 has been amended to clarify the City's intention that: Office development, together with further retail, conference and event spaces, will provide increased vibrancy and footfall to this part of the riverside. These uses would not be compatible with the delivery of a high quality residential environment and therefore residential development along this part of the river will be discouraged.</b></p>
12/47	DP9	Northern & Shell	<p>Paragraph 7.25.1 states that:</p> <p><i>"The primary planning aim for this stretch of the riverside is that it should provide office led commercial development with associated uses which serve the needs of the business City. This could include retail and conference or event space which would not be compatible with residential development"</i></p> <p>As per our comments above, we strongly disagree that retail, conference or event space would not be compatible with residential development. Retail floorspace at ground floor level above residential uses is a typical mixed use approach to development sites in Central London. Provided that there are sufficient controls on opening hours, servicing and deliveries, then <b>residential uses can sit comfortably alongside such retail conference or event uses.</b></p> <p>Again we do not consider that the SPD should be addressing land use issues and determining what is appropriate in land use terms for this part of the river – this is a matter for the Local Plan. We therefore consider <b>7.25.1 should be deleted.</b></p>	<p>The issue of land use along the riverside was considered at the Local Plan examination in October 2014. The Inspector's Report stated that "Policy CS9, which gives more emphasis to office-led commercial development along the Riverside, than was the case in the Core Strategy, is sound in view of the likely pressures for residential development. I am satisfied that the site specific viability considerations are able to be given due weight, and that the City Corporation has already demonstrated sufficient flexibility in that process. Therefore I do not support any different approach for the Thames Riverside."</p> <p>Since this Local Plan policy approach was found to be sound at examination, it would be inappropriate to alter it through a Supplementary Planning Document</p> <p><b>No change needed</b></p>
12/48	DP9	Northern & Shell	<p>In summary, we consider that the <b>location between London Bridge and All Hallows should be treated as one area</b> for the purposes of setting out guidance under the Thames Strategy. We are concerned that the Thames Strategy is being used as a tool to set out the City's <b>land use policy</b> approach to the riverside and determine what land uses are acceptable or not acceptable. This <b>is beyond the remit of an SPD</b> and we consider that</p>	<p>The Local Plan policy approach was found to be sound at the Local Plan Examination, it would be inappropriate to alter it through a Supplementary Planning Document.</p> <p><b>No change needed</b></p>

			the draft SPD should be amended accordingly. If you would like to discuss the above further, please contact Jonathan Marginson at this office.	
13/49	Anneli Harris	Office of the Rail Regulator	Thank you for consulting ORR on the above Thames Strategy Supplementary Planning Document. The Office of Rail Regulation has <b>no comment</b> to make on this particular document.	Comment noted <b>No change needed</b>
14/50	Lucy Owen	Port of London Authority	Thank you for consulting the PLA on the Thames Strategy Supplementary Planning Document Consultation Draft (January 2015). I have now had the opportunity to review the submitted documentation and would like to make the following comments:  1.5 Bullet point 2 – paragraph 1.4 refers to the river's functional uses in transport, navigation and recreation yet bullet point 2 only seeks to assist in the implementation of increased river transport. It is recommended <b>that bullet point 2 is widened to reflect the City of London's commitment to increased river transport, navigation and recreation.</b>	<b>Second bullet point has been amended as follows:</b> <ul style="list-style-type: none"> <li>Assisting the implementation of increased- <b>improved</b> river transport, <b>navigation and recreation opportunities</b></li> </ul>
14/51	Lucy Owen	Port of London Authority	1.5 Bullet point 6 – Walbrook Wharf is a safeguarded wharf and as such it is safeguarded for waterborne freight uses. As identified in para 7.20.2 its use as a waste transfer or waste management site is also protected and this is through the Local Plan. <b>Bullet point 6 needs to be updated to reflect this, it is suggested that the text is amended to read "and at Walbrook Wharf for waterborne freight traffic including waste management."</b>	<b>Bullet point 6 has been altered as follows</b> <b>...at Walbrook Wharf for <u>waterborne freight traffic including waste management</u>.</b>
14/52	Lucy Owen	Port of London Authority	2.2 There is <b>no reference to policy 7.26 of the London Plan</b>	Paragraph 7.20.2 includes a reference to policy 7.26 of the London Plan <b>No change needed</b>
14/53	Lucy Owen	Port of London Authority	2.3 It is recommended that <b>reference</b> is also made to the <b>Mayor's River Action Plan</b> and that the reference to the Mayor's Safeguarded Wharves Review is changed to the <b>Mayors Safeguarded Wharves Implementation Report (2005) and Safeguarded Wharves Review (2013)</b> . What is meant by the <b>Port of London Authority Strategy</b> ? It is noted that there is a reference to the "forthcoming" Marine Plan for the South East Inshore, the PLA is currently undertaking the <b>Thames Vision Project</b> and it is suggested that reference is made to this forthcoming Vision document.	Paragraph 2.3 includes reference to the Mayor's River Action Plan <b>Text has been altered to include reference to the Mayor's Safeguarded Wharves implementation Report (2005), the PLA's strategic priorities and the PLA's Thames Vision Project.</b> The Port of London Authority strategic priorities can be found at: <a href="http://pla.co.uk/assets/PLASummary.pdf">http://pla.co.uk/assets/PLASummary.pdf</a>
14/54	Lucy Owen	Port of London Authority	2.4.1 It is recommended that the word <b>"waste"</b> is removed from line 6 as this is covered by "freight" and that it is made clear that this includes during construction of new developments. Additionally, in order to ensure that the river can be utilised for freight, passenger transport, navigation and recreation it is recommended that a <b>reference is made in paragraph 2.4.1 to developments requiring a waterside location for river related use</b>	<b>"waste" deleted from paragraph 2.4.1</b> <b>Paragraph 2.4.1 has been amended to delete "waste" and to include the following phrase: .....ensuring that development on or over the river is restricted to uses which require a riverside location for a river related use.....</b>
14/55	Lucy Owen	Port of London Authority	3.1.1 The word <b>"Thames"</b> before river buses seems to be superfluous	<b>Paragraph 3.1.1 amended to delete "Thames" before river buses</b>
14/56	Lucy Owen	Port of London Authority	3.1.2 There is concern that the document is in effect advertising places where people can access the foreshore and that some of these places	<b>Proposed wording has been included at the beginning of paragraph 3.1.2. The list of access points has been retained but referred to as</b>

			have little or no foreshore due to the tides! The PLA cannot support the current wording of paragraph 3.1.2 and suggests that it is amended along the lines of the Hammersmith and Fulham Draft Local Plan: "Access to the foreshore, which is primarily in the ownership of the Port of London Authority, can have a recreational and educational value. There is a right of access for fishing, navigational and other customary purposes, but public access must be considered in the context of the environmental and archaeological importance of the foreshore and particularly safety considerations, bearing in mind the fast tidal flows affecting this part of the Thames."	historic access points which must be retained rather than points of access to the foreshore.
14/57	Lucy Owen	Port of London Authority	3.3.1 – the reference to developments on and over the river needing to have a river related use is welcomed and the text in chapter 7 relating to each part of the City's Thames Riverside should reflect this. For example, there is a reference at 7.20.3 to permanently moored vessels between Queenhithe and London bridge needing to be used for a river related purpose but this is the only reference I have been able to find	An additional paragraph 7.2a has been included in the introduction to Chapter 7: <u>Some issues apply to the whole of the Thames riverside within the City. For instance policy CS9.4(v) requires that proposed development on or over the river must require a riverside location for a river related use and CS9.4(vi) requires that permanently moored vessels must be of national importance, have a connection with the City and the River Thames, be used for a river related purpose and not have a detrimental impact on navigation, river regime or environment These requirements apply to all sections of the Thames Riverside....</u>
14/58	Lucy Owen	Port of London Authority	5.1.1 It would appear to be appropriate to include specific reference to lighting along the riverside and the need for it to be appropriately designed to minimise its impact on navigation and ecology.	New Paragraph 7.2a addresses this through inclusion of the following sentence: <u>... Similarly protection of the river's ecology and navigation must be considered in the design of lighting throughout the length of the City's riverside.</u>
14/59	Lucy Owen	Port of London Authority	5.2 Focal points of public activity – the references to Blackfriars Pier and Tower Millennium Pier are welcomed – it would be beneficial to explain here the targets set in the River Action Plan relating to increased passenger numbers. Technically, part of the upstream extension of Tower Pier is within the City.	Paragraph 5.2.4 has been amended with the inclusion of the following sentence: <u>Passenger numbers are likely to increase in line with targets in the Mayor's River Action Plan</u>
14/60	Lucy Owen	Port of London Authority	5.4 Inclusive design – it is recommended that the text on development and riverside walk enhancements includes a reference to designing them to include riparian life saving equipment	Paragraph 5.4.1 has been amended as follows: This includes provision of ramps or lifts adjacent to steps where there is a change in level between buildings and within the public realm, inclusive signage, tactile surfaces and consideration of soundscapes which aid orientation for people with visual impairments and the inclusion of riparian life-saving equipment.
14/61	Lucy Owen	Port of London Authority	5.5.2 See comments on 3.1.2 above. Paragraph 5.5.2 should be amended to the text set out in para 3.1.2. Whilst the reference to ladders and life belts are welcomed, reference should also be provided to grab chains.	Amended Paragraph 5.5.2 and 5.5.3 have been reversed and the new paragraph 5.5.3 has been amended as follows: There are steps down to the foreshore at the War Memorial on Victoria Embankment at Temple Gardens, Trig Lane, Cousin Lane, London Bridge and Custom House. Additionally there are private stairs at Vintners' Place and Fishmongers' Hall. These access points will be preserved and where possible made available for public access. Ladders are provided at intervals along the frontage for emergency access and egress to and from the river and foreshore. These ladders

				should be retained and maintained. Life belts <u>and grab chains</u> are provided in several locations along the foreshore and on some of the bridges for emergency use.
14/62	Lucy Owen	Port of London Authority	5.5.3 Technically walking on the foreshore is not permitted. Paragraph 5.5.3 needs to be <u>amended to the text set out in 3.1.2 above</u> . If the Council wishes to include a list of dangers then this should also <u>include wells disease and the composition of the foreshore</u> .	Paragraphs 5.5.2 and 5.5.3 have been reversed and the new paragraph 5.5.2 has been amended by inclusion of reference to <u>wells disease</u> and inclusion of the following sentence: <u>Access to the foreshore, which is primarily in the ownership of the Port of London Authority, can have a recreational and educational value. There is a right of access for fishing, navigational and other customary purposes, but public access must be considered in the context of the environmental and archaeological importance of the foreshore and particularly safety considerations.</u>
14/63	Lucy Owen	Port of London Authority	<u>5.5.4 It is recommended that the paragraph is amended</u> to read "The use of a metal detector and associated digging on the Thames foreshore requires a permit from the Port of London Authority. Digging on the north foreshore between Lambeth and Tower Bridges, which include the city's stretch of the foreshore, is restricted to members of the Thames Mudlark Society. Digging is also prohibited in several sections including Queenhithe Dock (see figure 6 for restricted/prohibited digging areas). All finds must be reported to the Museum of London. The Thames Discovery Programme and Foreshore Recording and Observation Group coordinate events on the Thames Foreshore. The Museum of London coordinates archaeological recording.	Text amended as suggested
14/64	Lucy Owen	Port of London Authority	<u>Figure 6 is also out of date</u>	Figure 6 updated with version dated 22/01/2014
14/65	Lucy Owen	Port of London Authority	5.6.3 <u>Remove "the freight movement facility at"</u> and start the sentence "Walbrook Wharf (a safeguarded wharf) is currently used as a waste transfer station. <u>It should be made clear the temporary pontoons are not used at Walbrook Wharf but rather within the City's boundary</u> to enable the transport of construction and waste materials. The PLA would like to work with the City and developers to identify more opportunities to use the river during construction.	Paragraph 5.6.3 has been amended as follows: <u>5.6.3 The freight movement facility at Walbrook Wharf (a safeguarded wharf) is currently used as a waste transfer station. Temporary pontoons elsewhere in the river within the City's boundary, are used on occasions to enable the transport of construction materials and waste by river from development sites (e.g. Blackfriars Station redevelopment and Thames Tideway Tunnel Infrastructure Project).</u>
14/66	Lucy Owen	Port of London Authority	New paragraph 5.6.5 It is recommended that a <u>new 5.6.5 emphasises the point made later at 5.8.2</u> that in order to ensure that the river can be used for its transport, navigation and recreation functions policy 9.4v is of particular important in refusing development on or over the river which does not require a river location.	New paragraph 5.6.5 has been added: <u>5.6.5 In order to ensure that the river can be used for its transport, navigation and recreation functions Local Plan policy CS9.4 (v) is of particular important in refusing development on or over the river which does not require a waterside location for river related uses</u>
14/67	Lucy Owen	Port of London Authority	5.8.1 Just under <u>1000 vessels took part in the Queens Jubilee River Pageant</u>	Text amended to read <u>...almost 1,000 vessels took part in the Queens Jubilee River Pageant</u>
14/68	Lucy Owen	Port of London Authority	5.9 It would be useful to review the <u>evidence</u> that has been collected to enable the City Corporation to reach the conclusion <u>that there are no deficiencies in river related facilities</u> . It is also recommended that the text is updated to identify that the <u>situation will be kept under review</u> and that support will be given to proposals for river related facilities.	Paragraph 5.9.2 has been amended as follows: The City Corporation has not identified any deficiencies in such facilities for the stretch of the Thames which passes through the City of London. <u>This situation will be kept under review and support will be given to proposals for appropriate river related facilities</u>
14/69	Lucy Owen	Port of London	6.2.3 It is important to note that all works in the river require a <u>river works</u>	The following sentence has been added to paragraph 6.2.3:

		Authority	licence from the PLA and that works need to be kept in a good state of repair.	<b>All works in the river require a river works licence from the PLA and that works need to be kept in a good state of repair.</b>
14/70	Lucy Owen	Port of London Authority	7.2 Reword to read "River related issues and challenges include the safeguarding of Walbrook Wharf for waterborne freight uses..."	Paragraph 7.2 has been reworded as follows: River related issues and challenges include the safeguarding of Walbrook Wharf for waterborne freight uses as a working river wharf....
14/71	Lucy Owen	Port of London Authority	7.19 the retention of a "viable wharf" is essential	The third bullet point of paragraph 7.2.1 has been amended to read: At Walbrook Wharf, the retention of a viable wharf with river access and vessel manoeuvring space is essential
14/72	Lucy Owen	Port of London Authority	7.20.3 It is questioned why the reference to permanently moored vessels is only made here. Will they be acceptable to the Council in all the other areas?	An additional paragraph 7.3 has been included in the introduction to Chapter 7: Some issues apply to the whole of the Thames riverside within the City. For instance policy CS9.4(v) requires that proposed development on or over the river must require a riverside location for a river related use and CS9.4(vi) requires that permanently moored vessels must be of national importance, have a connection with the City and the River Thames, be used for a river related purpose and not have a detrimental impact on navigation, river regime or environment These requirements apply to all sections of the Thames Riverside....
15/73	Savills	Thames Water	<p>Thank you for consulting Thames Water Utilities Ltd. (Thames Water) on the above. Thames Water is the statutory sewerage undertaker and statutory water undertaker for City of London and the following comments are made in this respect. Thames Water Utilities Ltd (Thames Water) Property Services function is now being delivered by Savills (UK) Limited as Thames Water's appointed supplier. Savills are therefore pleased to respond to the above consultation on behalf of Thames Water.</p> <p>Thames Water fully supports the preparation of the Thames Strategy Supplementary Planning Document (SPD), but would like to make the following comments with respect to the Thames Tideway Tunnel, which is partially within the Thames Strategy area (as shown in Figure 1 of the draft SPD).</p> <p>Thames Tideway Tunnel.</p> <p>The Thames Water Utilities Limited (Thames Tideway Tunnel) Order 2014 as amended by The Thames Water Utilities Limited (Thames Tideway Tunnel) (Correction) Order 2015 (the DCO (as amended)) provides consent for the construction and operation of a wastewater storage and transfer project of national significance, known as the Thames Tideway Tunnel. This consent is a material consideration of substantial weight in the consideration of all projects, plans and proposals in the vicinity of the project. The National Policy Statement for Waste Water and the Examining Authority's Report make clear that the Government considers that there is an established, urgent need for this nationally significant infrastructure.</p>	<p>Support noted</p> <p><b>No change needed</b></p>

			The tunnel alignments and sites that form part of the Thames Tideway Tunnel are safeguarded by means of Article 52 of the Order and requires that in determining any planning application within the TTT safeguarded area, the local planning authority must have regard to any recommendation received by Thames Water. <b>Thames Water support the inclusion of the Thames Tideway Tunnel Safeguarding Area shown in Figure 1 of the draft SPD.</b>	
15/74	Savills	Thames Water	In order to accurately describe the Thames Tideway Tunnel development, Thames Water suggest the following minor amendments to the SPD:  <b>Replace the 2nd sentence in paragraph 5.2.1</b> with the following:  "Development Consent for the Thames Tideway Tunnel nationally significant infrastructure project was granted in September 2014 and will address the issue of London's combined sewer overflows into the River Thames"	<b>Text amended to read</b> <b><u>Development Consent for the Thames Tideway Tunnel nationally significant infrastructure project was granted in September 2014 and will address the issue of London's combined sewer overflows into the River Thames</u></b>
15/75	Savills	Thames Water	Thames Water support the issues stated in paragraph 6.4.3 and <b>suggest the following sentence is added to the end of paragraph 6.4.3:</b>  "The recently consented Thames Tideway Tunnel will address London's combined sewer overflows into the River Thames."	<b>The following sentence has been added to paragraph 6.4.3</b> <b><u>The recently consented Thames Tideway Tunnel will address London's combined sewer overflows into the River Thames</u></b>
15/76	Savills	Thames Water	Thames Water welcome the inclusion of Thames Tideway Tunnel in section 7.4 of the SPD. However, TWUL suggest <b>the first sentence under bullet 2 is replaced with the following text:</b>  "this nationally significant infrastructure project is a major new sewer, urgently needed to protect the tidal River Thames from increasing pollution. The project comprises a wastewater storage and transfer tunnel designed to intercept identified combined sewer overflows (CSOs) that frequently discharge into the tidal reaches of the River Thames."	<b>First bullet point in paragraph 7.4 has been amended to read</b> <b><u>This nationally significant infrastructure project is a major new sewer, needed to protect the tidal River Thames from increasing pollution. The project comprises a wastewater storage and transfer tunnel designed to intercept identified combined sewer overflows (CSOs), including one at Blackfriars, which frequently discharge into the tidal reaches of the River Thames. ...</u></b>
15/77	Savills	Thames Water	Paragraphs 7.5.3 and 7.7.1 of the draft SPD make reference to possible design features for the Project's Blackfriars Bridge Foreshore site. The application for development consent included an indicative level of design detail, framed within a series of parameters and design principles. <b>The detailed design has not yet been developed. Future discussions with the City of London will be valuable in developing the design</b> and ensuring matters relating to flood risk, public realm, landscaping and drainage are addressed appropriately, proportionately, and in the context of the functional requirements of the Project.  Thames Water would be glad to work with the City of London to develop the Supplementary Planning Document.  I trust the above is satisfactory, please do not hesitate to connect me should you require any further information.	Support for ongoing discussions regarding the Blackfriars Bridge foreshore site noted. <b>No change needed</b>

16/78	Mark Day	Transport for London	The Thames Strategy is welcomed although there is a very <b>minor error at paragraph 5.2.4</b> of the Thames Strategy where the first sentence should read "Tower Millennium Pier is used by river bus services, river tour services and private-chartered boats."	<b>Text amended as suggested</b>
17/79	Collette Willis	Westminster City Council	<p>Thank you for the opportunity to comment on the consultation draft Thames Strategy SPD</p> <p>I welcome the acknowledgment of the need for consistency with neighbouring boroughs on the approach to the riverside, and welcome coordination on cross-borough issues.</p> <p>There are several <b>views originating in Westminster</b> which take in the City's part of the river and riverside townscape. There are <b>four draft Metropolitan Views</b> (locally designated views considered to be of Metropolitan Importance). These are:</p> <ul style="list-style-type: none"> <li>• View downstream from Westminster Bridge (draft Metropolitan View 44)</li> <li>• View downstream from Golden Jubilee Footbridge (draft Metropolitan View 43a)</li> <li>• View downstream from Waterloo Bridge (draft Metropolitan View 42a)</li> <li>• St Pauls from Somerset House Terrace (draft Metropolitan View 25)</li> </ul> <p>There are also 'local views' which are identified in our conservation area audits. Of particular significance are the <b>views contained in the audits for the Savoy and Strand conservation areas</b>.</p> <p>Westminster trust that impacts on these views would be considered in development proposals adjacent to the River in the City of London, and we would welcome acknowledgement of this in your SPD.</p>	<p><b>Paragraph 4.1.4 has been added:</b>  <b>4.1.4 There are several views originating in Westminster which take in the City's part of the river and riverside townscape. These include downstream views from Westminster Bridge, Golden Jubilee (Hungerford) Bridge and Waterloo Bridge. Development proposals near the river should consider the impact on these views.</b></p> <p>Other draft views referred to are not considered to directly affect the City's Thames Strategy area.</p>
17/80	Collette Willis	Westminster City Council	An <b>update at Paragraph 2.7.2</b> is required – it refers to the 'City of Westminster Core Strategy' this should refer to the adopted plan the <b>'Westminster City Plan: Strategic Policies'</b>	<b>Paragraph 2.7.2 has been amended to read:</b> <b>The Westminster City Plan: Strategic Policies document identifies the Thames policy area.....</b>

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City of London

# Thames Strategy

Supplementary Planning Document  
Draft for adoption

June 2015



## Consultation

The City of London Corporation (City Corporation) consulted on a draft Thames Strategy Supplementary Planning Document (SPD) between 18<sup>th</sup> March 2015 and 30<sup>th</sup> April 2015 in accordance with the procedures set out in The Town and Country Planning (Local Planning) (England) Regulations 2012 and the requirements of the City Corporation's Statement of Community Involvement 2012.

This draft for adoption has taken account of the consultation comments made and is recommended for adoption. A consultation statement has been published outlining how the City Corporation has taken account of the comments made.

Copies of the documents are available:

Online at: [www.cityoflondon.gov.uk](http://www.cityoflondon.gov.uk)

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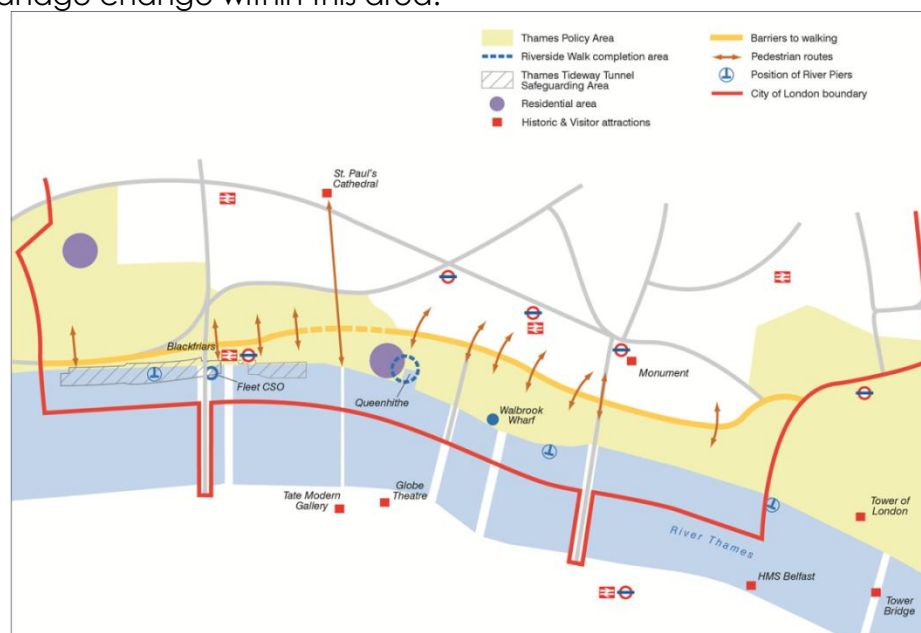
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# 1 Introduction and objectives of the Thames Strategy

1.1 The London Plan requires Thames-side boroughs and the City Corporation to identify a Thames Policy Area and formulate policies and a strategy for this area. The City's part of the Thames Policy Area is identified in the Local Plan and on the Local Plan Policies Map. "Thames and the Riverside" is identified as a Key City Place in the Local Plan (Fig 1) and this Strategy will help to manage change within this area.



**Figure 1 Thames and the Riverside Key City Place**

1.2 The City Corporation adopted a Riverside Appraisal of the Thames Policy Area in the City of London in 2002 as Supplementary Planning Guidance (SPG) to the City's Unitary Development Plan 2002. Recent developments on the riverside and the Local Plan policy for Thames and the Riverside (CS9) necessitate a review of this riverside appraisal. The revised document is being re-named the Thames Strategy, in accordance with London Plan Policy 7.29, and will be adopted as a Supplementary Planning Document (SPD) to replace the 2002 SPG.

1.3 The Thames Policy Area spans a number of City of London wards therefore this strategy will impact to some degree on the following wards: Farringdon Without, Castle Baynard, Queenhithe, Vintry, Dowgate, Bridge, Billingsgate and Tower.

1.4 The riverside location of the City of London has shaped the City throughout its history and continues to present opportunities and challenges for the spatial planning of this area into the future. This strategy looks forward to 2026 identifying the issues that the riverside might face. The aim is that the City should capitalise on its unique riverside location sustaining the river's functional uses in transport, navigation and recreation whilst minimising the risks to the City's communities from flooding.

1.5 The key objectives of this strategy are to provide guidance on:

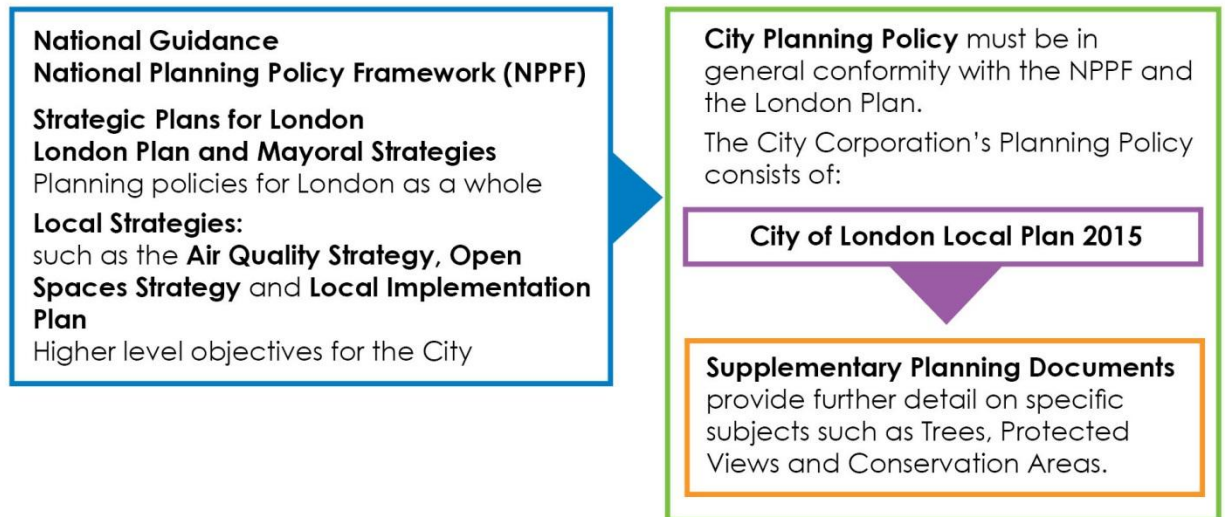
- Development and public realm enhancement within the Thames Policy Area;

- Assisting the implementation of improved river transport, navigation and recreation opportunities;
- Protection and enhancement of heritage assets;
- Inclusive access for all wherever practicable;
- Flood risk, climate resilience and biodiversity enhancement;
- The implications for development of site safeguarding at Blackfriars for the Thames Tideway Tunnel and at Walbrook Wharf for waterborne freight traffic including waste management.

Balancing and reconciling potentially conflicting interests arising from these objectives is an important intention of this strategy.

## 2 Policy context

2.1 The National Planning Policy Framework sets out the national planning policies for England and how they are expected to be applied. Planning for coastal and tidal waters is set out in Marine Plans and the Thames River Basin Management Plan. The relevant local policies and designations are contained in the City of London Local Plan which must be in general conformity with strategic policies in the London Plan.



2.2 Development on the riverside must have regard to the strategic policies in the London Plan including the following policies which have particular relevance for the Thames Policy Area:

- Policy 5.10 Urban Greening
- Policy 5.12 Flood Risk Management
- Policy 5.13 Sustainable Drainage
- Policy 5.14 Water Quality and Waste Water Infrastructure
- Policy 5.15 Water use and supply
- Policy 6.10 Walking
- Policy 6.11 Smoothing traffic flow and tackling congestion
- Policy 6.14 Freight
- Policy 7.2 An inclusive environment
- Policy 7.5 Public Realm
- Policy 7.8 Heritage Assets and Archaeology
- Policy 7.11 London View Management Framework (LVMF)
- Policy 7.14 Improving Air Quality
- Policy 7.19 Biodiversity and access to nature
- Policy 7.24 The Blue Ribbon Network
- Policy 7.25 Increasing the use of the Blue Ribbon Network
- Policy 7.27 Supporting infrastructure and recreational use
- Policy 7.28 Restoration of the Blue Ribbon Network
- Policy 7.29 The River Thames

2.3 Other relevant regional strategies include:

- The Environment Agency's Thames Estuary 2100 Plan (TE2100)
- Port of London Authority Strategic Priorities and Thames Vision Project

The Mayor's River Action Plan  
 Mayor's Transport Strategy.  
 Mayor's Safeguarded Wharves Implementation Report (2005)  
 Mayor's Safeguarded Wharves Review (2013)  
 Marine Plan for the South East Inshore (forthcoming)  
 The Environment Agency's Thames River Basin Management Plan

2.4 The City of London Local Plan provides specific policies which will guide development in the Thames Policy Area:

2.4.1 Policy CS9 *Thames and the Riverside* sets out the City's intentions for the Thames Riverside within the City, promoting office led commercial development on the riverside, requiring that development on the riverside contributes to the aims of the Riverside Walk Enhancement Strategy, ensuring that development on or over the river is restricted to uses which require a riverside location for a river related use, supporting the construction of the Thames Tideway Tunnel and promoting functional uses of the river for freight and passenger transport, navigation and recreation.

2.4.2 Policy CS10 *Design* requires that the design of spaces should provide for the access needs of all the City's communities and promotes the City's public realm enhancement strategies.

2.4.3 Policy CS12 *Historic Environment* sets out the requirements for protection and enhancement of the historic environment including conservation areas, listed buildings and structures and archaeological assets. The Temples, Whitefriars, Trinity Square and Crescent Conservation Areas, Baynard's Castle and Queenhithe Dock Scheduled Ancient Monuments and numerous listed buildings & structures fall within the Thames Policy Area. The Thames foreshore is of particular archaeological significance.

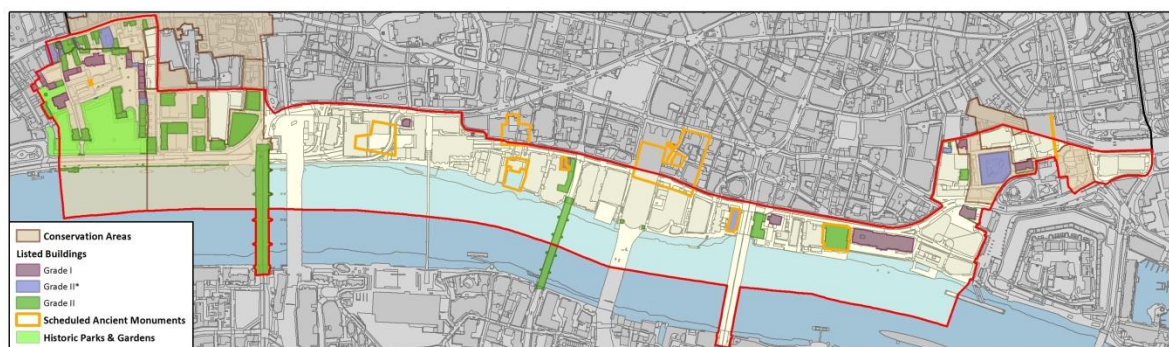


Figure 2 Heritage Assets in the Thames Policy Area

2.4.4 CS13 *Protected Views* protects strategic and local views of St Paul's Cathedral, the Tower of London and the Monument. It also protects the setting of the Tower of London World Heritage Site.

2.4.5 Policy CS17 *Waste* safeguards Walbrook Wharf and promotes the use of the Thames for the transport of waste and construction materials.

2.4.6 Policy CS18 *Flood Risk* aims to ensure that the City remains at low risk from all types of flooding. Most of the Thames Policy Area falls within the City Flood Risk Area and is susceptible to fluvial flood risk through breach

or overtopping of the flood defences and to surface water /sewer flood risk through overloading of the combined sewer network in the area.

2.4.7 Policy CS 19 *Open Spaces and Recreation* encourages improved access to open space aiming to increase the amount and quality of open space and green infrastructure while enhancing biodiversity. It also identifies that the River Thames is a Site of Metropolitan Importance for Nature Conservation (SMINC).

2.4.8 Core strategic policies are supported by Development Management policies which provide further guidance to determine planning decisions.

2.5 Other City of London Strategies. The City Corporation has adopted a series of strategies which are relevant to the Thames Riverside including:

- Riverside Walk Enhancement Strategy 2014
- Local Flood Risk Management Strategy 2014-2020
- City of London Waste Strategy 2013-2020
- City of London Air Quality Strategy 2011-2015
- City of London Visitor Strategy 2013-17
- City of London Cultural Strategy 2012-2017
- City of London Open Space Strategy 2015-2020

2.6 The City Corporation is the Port Health Authority for the tidal Thames dealing with health issues associated with the river and river craft.

2.7 Neighbouring boroughs' Local Plans

2.7.1 The Tower Hamlets Core Strategy identifies locally distinctive places across the whole borough. Along the riverside eight places are identified, the nearest three to the City are Tower of London & St Katherine's Dock, Wapping and Limehouse. The riverside policy area is designated in these places and policies promote protection of the settings of the Tower of London, extension of the riverside walk, the development of mixed use areas including employment, residential and complementary leisure uses.

2.7.2 The Westminster City Plan: Strategic Policies document identifies the Thames policy area and requires that developments in this area will need to demonstrate that they have particular reference to their riverside location and local architectural references, including long views of the riverside.

2.7.3 The Southwark Core Strategy – includes a vision for Bankside & London Bridge. This includes protection of access to and along the riverside and enhancement of the character of the riverside, encouragement for river transport, recognition of the importance of the Thames bridges, protected views and flood risk policies and identification of a Thames Policy Area. Preparation of a Bankside Neighbourhood Plan is underway.

2.7.4 The Lambeth Core Strategy identifies the Thames Policy Area to protect and enhance the special character of the River Thames and Thames-side.



2.7.5 The City Corporation is keen to ensure that this Strategy forms part of a comprehensive approach to the central London's riverside and will be consistent with neighbouring boroughs' strategies.

### 3 Local character

#### 3.1 The River today

3.1.1 The River Thames flows through the City from its boundary with Westminster to the west of Blackfriars Bridge, to its boundary with Tower Hamlets near Tower Bridge. The river is tidal, with a tidal range of seven metres twice per day, throughout this section. Flood defences and river walls protect riverside properties and public realm at high tide. At low tide wide areas of foreshore are visible at several locations along the river within the City. The natural extent of the river has been extensively modified and confined due to the extension of quays, wharves and buildings into the river. Six bridges span the river within the City (Blackfriars road bridge, Blackfriars Railway Bridge, Millennium footbridge, Southwark Bridge, Cannon Street Railway Bridge and London Bridge). The navigable river channel is used for a variety of vessels including barges towing waste and construction materials, river buses, leisure boats and river-patrol boats. The ecology of the river has improved in the last 50 years and it is now designated as a Site of Metropolitan Importance for Nature Conservation.

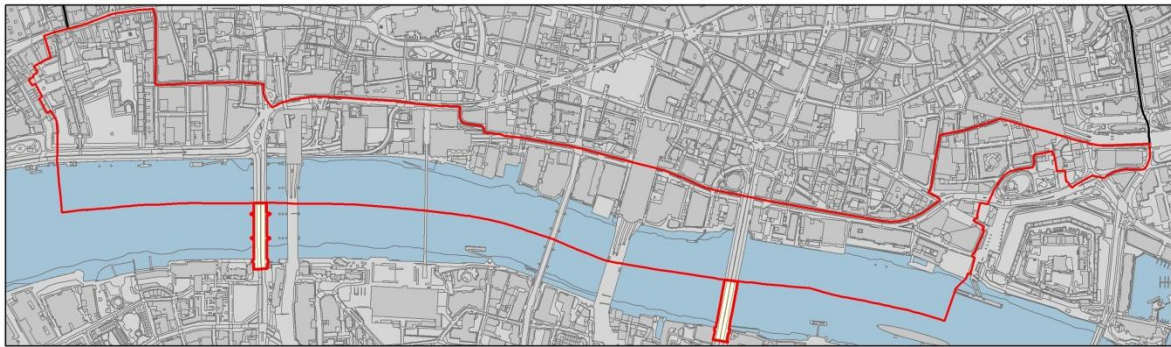


Figure 3 Thames Policy Area

3.1.2 Access to the foreshore, which is primarily in the ownership of the Port of London Authority, can have a recreational and educational value. There is a right of access for fishing, navigational and other customary purposes, but public access must be considered in the context of the environmental and archaeological importance of the foreshore and particularly safety considerations, bearing in mind the fast tidal flows affecting this part of the Thames. Historic access points next to the War Memorial on Victoria Embankment at Temple Gardens, Trig Lane, Cousin Lane, London Bridge and Custom House, and private stairs at Vintners' Place and Fishmongers' Hall should be retained. Ladders are available at intervals along the frontage for emergency egress.

3.1.3 The flood defences vary in character along the Thames but all provide protection to at least the statutory flood defence level of 5.41m AOD. Environment Agency inspection, as reported in the Strategic Flood Risk Assessment 2012, confirms that the majority of the City's flood defences are in good condition, with a small stretch at Three Quays classified as 'fair'.

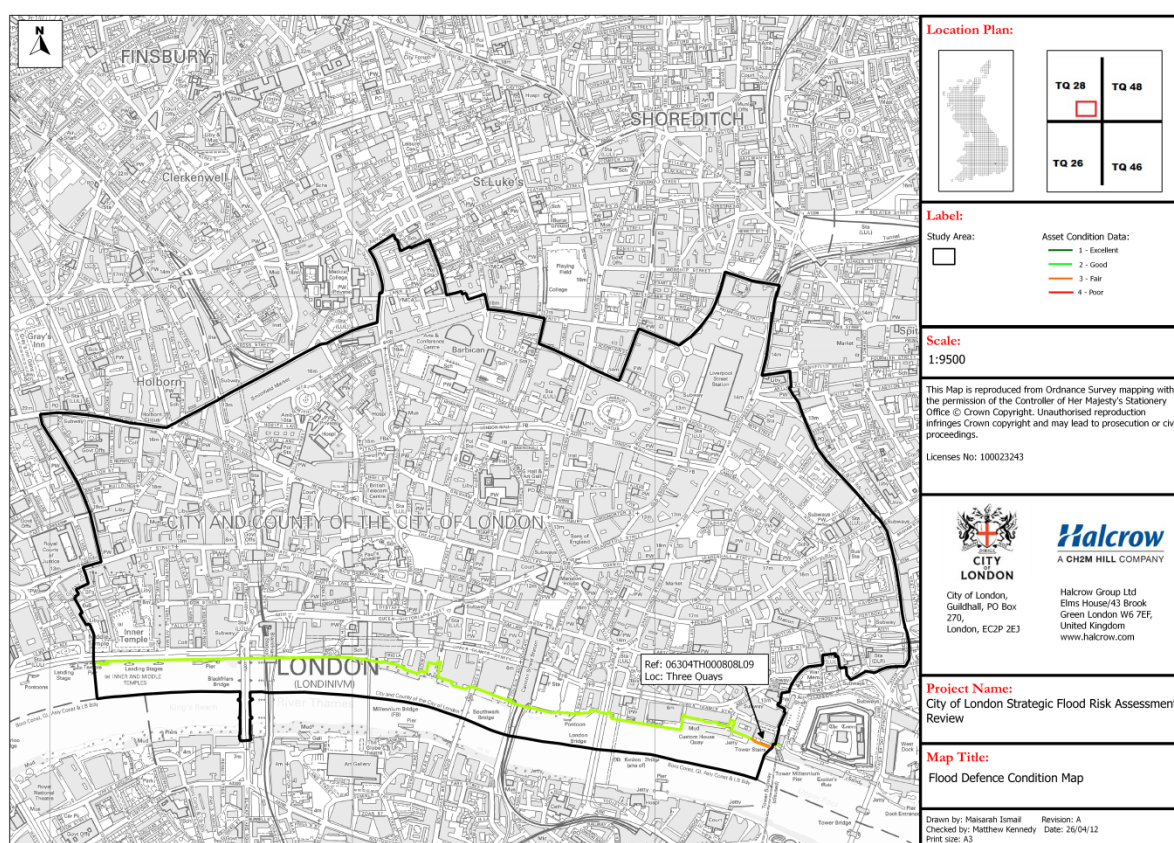


Figure 4 Flood defence condition

3.1.4 The Thames Estuary 2100 Plan identifies actions that will need to be taken during this century to protect the land adjacent to the tidal Thames from flood risk. This area, including parts of the City, is protected from tidal flooding by the Thames Barrier. However climate change impacts such as sea level rise and the prevalence of more extreme weather events mean that additional protection will be needed later this century. Within the City this will include the need to raise flood defences by up to 0.5 metre by 2065 and a further 0.5m by 2100. This will have an impact on riverfront structures and on views of the river from the riverside walk and from buildings adjoining the river.

3.1.5 In the west of the City the riverside is dominated by Victoria Embankment which is a busy dual carriageway. Although there is a wide tree lined pavement between the road and the river and Inner and Middle Temple Gardens to the north, the presence of this through route for traffic creates a noisy environment. To the east of Blackfriars Bridge the road enters Blackfriars underpass for a short distance and then continues through the remainder of the City to the north of the river along Upper Thames Street and Lower Thames Street. As a result of this the riverside from Blackfriars Bridge to the eastern City boundary at Three Quays is predominantly a pedestrian route.

3.1.6 In recent years several developments have been completed or permitted along the Thames riverside in the City. At the eastern end overlooking the Tower of London a 77 bedroom hotel with 64 residential flats has been developed at Three Quays. At Sugar Quay permission has been granted for a residential development of 165 flats. High specification

office developments, Watermark Place and Riverbank House, between London Bridge and Cannon Street Railway Bridge, were completed in 2010. The residential area at Queenhithe has expanded with the addition of Sir John Lyon House and Globe View which created a cluster of around 700 residential flats. Development associated with Cannon Street Railway Station and Blackfriars Station has improved transport infrastructure. At Blackfriars Station the Thameslink work includes platforms spanning the railway bridge allowing for longer trains than would otherwise have been possible.

- 3.1.7 The cumulative impact of this new development and associated public realm enhancements is to make the City's riverside an extremely desirable and inclusive location with pressure for residential and hotel development vying with commercial development proposals.

### **3.2 Historic development of the riverside**

- 3.2.1 The historical development of the City is closely bound with its location on the Thames. The City's primacy as a centre of commerce and finance owes its origins to its role as a port.
- 3.2.2 Roman Londinium was founded on ground above the flood plain at a point where the Thames could be bridged. Although not at first the capital of Britannia, London quickly became its most important trading centre. The river shore, which at that time was on the line of the present Upper Thames Street, was lined with wharves and warehouses; the scale of these, as revealed by archaeological excavations, demonstrates the port's considerable prosperity. Wharf construction started a process of reclamation that led to the continual narrowing of the river over the next two millennia. The River Fleet, which joined the Thames where Blackfriars Bridge now stands, ran outside the western wall of the Roman settlement and was navigable as far as the present Ludgate Circus.
- 3.2.3 Following the end of Roman rule the walled city was largely abandoned and a new Saxon trading settlement called Lundenwic grew up around the Strand. Viking raids in the ninth century caused the settlement to move back within the Roman walls. Defences were repaired and strengthened, including a wall along the River, the bridge was repaired and a new planned street network was laid out. As wharves were incrementally extended into the river a series of closely-spaced lanes and alleys running down to the water front developed between them, some of which remain today. Docks were created at Queenhithe and Billingsgate, the former still visible as an inlet.
- 3.2.4 The Normans built the Tower of London beside the River to establish their dominance over London. During the twelfth century the river wall was demolished and Thames Street was constructed to give easier access to the wharves. London Bridge was rebuilt in stone, remaining the only crossing of the Thames in the capital for nearly six centuries. The narrow arches of the bridge restricted the movement of ships, confining much activity to the Pool of London, downstream of the crossing, which by the end of the Middle Ages handled half of the nation's trade. The riverside in the west of the City was lined with royal and ecclesiastical buildings,

including those of the Blackfriars, Whitefriars and Knights Templar, their grounds running down to the water front.

3.2.5 The Hanseatic League, a trading partnership which extended across much of northern Europe, had a presence at Steelyard from the fifteenth to the late sixteenth century. Distinctive coin-like jettons used by the Hansa for accounts and lead seal hallmarks for wool have been excavated. Remains of the Guildhall of the merchants of Cologne, documented in London from 1175, and buildings associated with traders of the Hanseatic League have been recorded below Cannon Street Station.

3.2.6 Most of the medieval City was destroyed in the Great Fire of 1666. The rebuilding took full advantage of the City's location on high ground, creating a striking skyline seen from the riverside that was dominated by St Paul's Cathedral surrounded by 51 church towers and spires. The river fulfilled a ceremonial role, used for water-borne processions of the City and the Crown. River trade continued to be confined within the City's boundaries, its volume tripling by the eighteenth century to make London the world's busiest port. Banking, insurance, warehousing and related services developed to support the trade of the port, establishing the City's position as a financial centre. From the early nineteenth century the construction of large, purpose-built docks downstream led to the reduction of trade in the Pool; although by the 1890s there were still 50 working wharves on the City waterfront, these now handled specialised and fresh produce.

3.2.7 During this period there was much rebuilding along the river. In the eighteenth century the Fleet was covered over and New Bridge Street constructed above it. Blackfriars Bridge was built in 1769 and Southwark Bridge in 1819. Old London Bridge was famous for the houses standing upon it, but by the eighteenth century these had been removed in the interests of improvement. In 1831 the bridge was demolished and a new London Bridge built on a site further upstream; King William Street was constructed to connect to the changed alignment. The river shore between Blackfriars and Westminster was reclaimed for the construction of the Victoria Embankment, which provided an interceptor sewer and underground railway beneath a new road. Queen Victoria Street was cut through the existing street network to carry traffic from the Embankment onwards to Bank. Blackfriars Bridge was rebuilt in iron in 1869, Tower Bridge built in 1894 and Southwark Bridge rebuilt in 1921. Rail bridges were constructed at Blackfriars in 1864 and 1886, and Cannon Street in 1866, the stations forming new riverside landmarks.

3.2.8 The Victorian form of the City's river frontage remained largely unchanged during the first half of the twentieth century. East of Blackfriars Bridge the river was lined with wharves and warehouses, shipping continued to deliver goods and there was still much warehousing and manufacturing throughout the City alongside its now-dominant financial activities.

3.2.9 Post-war reconstruction led to radical changes to the riverside from the late 1940s onwards. River-borne trade dwindled as riverside warehousing and wharves were redeveloped for office buildings. Sites were

amalgamated, absorbing numerous lanes, alleys and stairs that formerly gave access to the riverside. Upper and Lower Thames Streets were considerably widened to form a dual carriageway. This route was continued westwards by the construction of the Blackfriars underpass on land reclaimed from the river. The underpass was decked over and a new pedestrian boulevard created to form a vista of St Paul's Cathedral from the river; this route was later extended to the south bank by the construction of the Millennium Bridge. As riverside sites were redeveloped a riverside walk was incorporated into each, eventually linking to form a continuous pedestrian route. In the 1980s a river bus service was introduced, with piers at Blackfriars and the Tower. An improved dock was built at Walbrook Wharf in the 1990s for the transfer of the City's waste onto barges; with the relocation of Billingsgate fish market in 1982 this remains the only river-related cargo handling activity in the City.

### **3.3 Future issues.**

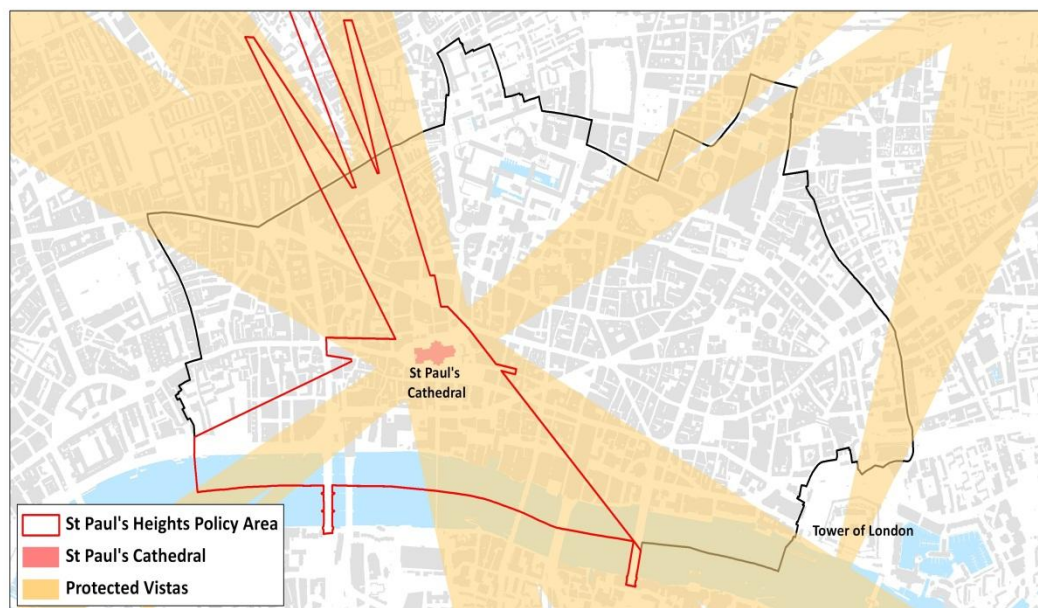
3.3.1 The City's riverside has experienced constant change throughout its history and the challenges to be faced in the future will bring further change in the balance of activities that take place there. Particular challenges which this strategy must address are:

- Conflicting development pressures – particularly pressure for residential development on the riverside competing with commercial, particularly office, uses which support the business City.
- Pressure for development on or over the river and the challenge of ensuring that such development is essential to the functioning of the river and constitutes river related uses.
- Opportunities for conservation of heritage assets, ensuring that new development does not detract from these assets and their settings.
- Climate change impacts – particularly increased flood risk as a result of sea level rise and greater frequency of more extreme weather events.
- Increased numbers of pedestrians as a result of transport improvements and improved accessibility to tourist attractions.



## 4 Areas, structures and landscapes of particular sensitivity

### 4.1 St. Paul's Heights and other local views



**Figure 5 Protected Views**

- 4.1.1 St. Paul's Heights limitations have been employed for over 70 years to protect and enhance the local views of the Cathedral for the enjoyment of Londoners and visitors alike. It is implemented through policy CS13 of the City of London Local Plan. The St. Paul's Heights operate by defining a series of inclined planes between the viewpoints and the Cathedral which form a ceiling on development through which no building can be allowed to rise if the views are to be preserved. The viewpoints protected by the St. Paul's Heights are from the Thames bridges, the south bank between Hungerford Bridge and London Bridge, and a number of views from streets to the west, north and east.
- 4.1.2 The St. Paul's Heights policy area covers the Thames Policy Area of the City from the City's western boundary with Westminster at Victoria Embankment, to the riverside walkway at Hanseatic Walk. The boundary of the St. Paul's Heights policy area extends to the southern end of London Bridge and Blackfriars Bridge. Many of these protected views are from viewpoints in Westminster, Lambeth and Southwark.
- 4.1.3 Development or redevelopment proposals must conform to the Heights limitations. The views protected by the Heights are sensitive to even small infringements. Consistent application of the limitations is therefore crucial to successful protection of the views. For more information relating to St. Paul's Heights, please view the City of London Protected Views Supplementary Planning Document (2012).
- 4.1.4 There are several views originating in Westminster which take in the City's part of the river and riverside townscape. These include downstream views from Westminster Bridge, Golden Jubilee (Hungerford)

Bridge and Waterloo Bridge. Development proposals near the river should consider the impact on these views.

#### **4.1.5 Tower of London Local Setting**

4.1.6 The Tower of London Local Setting is defined in the Tower of London World Heritage Site Management Plan (2007) as comprising “the spaces from which it can be seen from street and river level, and the buildings that provide definition to those spaces”. The Tower of London Local Setting Study (2010) sets out aims and objectives for conserving, promoting and enhancing appreciation of the Outstanding Universal Value of the Tower of London World Heritage Site, within a defined local setting area.

4.1.7 The study includes a series of suggested “approach routes”, based on the main historic routes to and from the Tower and surrounding urban environment. Movement along these routes offers the potential for pedestrians to appreciate the Outstanding Universal Value of the Tower from varying distances. In addition it provides for ease of access and understanding of the public realm relating to the World Heritage Site. several of these routes can be experienced in the Thames Policy Area:

- Route 2: Three Quays Walk (the aim is to create a high quality route from which pedestrians can experience the significance of the Tower's relationship to the river to the south and the City to the North).
- Route 3: Lower Thames Street (the aim is to create a high quality approach that allows pedestrians to appreciate the Outstanding Universal Value of the Tower, especially its Norman military architecture and concentric castle design).
- Route 4: Great Tower Street (the aim is to create an approach which reflects the significance of the relationship between the City of London and the Tower and maintains the open qualities of the threshold to Tower Hill, from where the Tower appears dominant in its setting as a recognisable landmark, and the defences are visible as a symbol of its military architecture).
- Route 5: Savage Gardens – aim is to create an approach that reflects the significance of the relationship between the Tower and the City and emphasises the threshold between the street within the local setting and the “Liberties”, retaining and enhancing views of the Tower.
- Route 6: Cooper's Row – aim is to create an approach that reflects the significance of the relationship between the Tower and the City and celebrates the Tower as a main focus point.
- Route 8: Vine Street – aim is to create views which facilitate people's appreciation of the Tower of London World Heritage Site
- Route 9: Minories – aim is to create an approach which celebrates the Tower as a main focus, revealing the prominent military architecture and outstanding example of concentric castle design.



4.1.8 The Tower of London Local Setting Study provides further detail on representative viewpoints that exemplify the Outstanding Universal Value of the Tower of London. Development in the Thames and Riverside could affect some of these views and any potential impact should be assessed through Townscape and Heritage Impact Assessment. The relevant views are:

- View 5: Main entrance to the Tower
- View 7: Wakefield Gardens (above Tower Hill underground station)
- View 9: Tower Bridge
- View 10: Queen's Walk
- View 11: London Bridge

## 4.2 Monument Views

4.2.1 The protected views from the gallery of the Monument are shown on Policies Map A of the City of London Local Plan. The views are protected by policy CS13 of the Local Plan and explained further in the City of London Protected Views Supplementary Planning Document (SPD) 2012. The policy protects and enhances significant local views of and from the Monument.

4.2.2 The Thames Policy Area is within four of the City's five views from the Monument. These are as follows:

- View One: South East to the Tower of London, Tower Bridge, the River Thames and HMS Belfast
- View Two: South to the River Thames
- View Three: South West to London Bridge and Cannon Street Railway Bridge
- View Four: West to Waterloo Bridge and Victoria Embankment

4.2.3 The height and massing of development should not visually intrude into the key features of the views, and views of the River Thames should be maintained.

4.2.4 The immediate setting of the Monument is formed by four surrounding street blocks defined on the Policies Map. Development in these four street blocks should not impinge on the general open character of the space around the gallery and should not detract from the elevation of the Monument in relation to its surroundings. The view from Queen's Walk (western end) northwards to the Monument is important to the Thames and Riverside area because it provides the most complete and intimate view of the Monument from the south bank and from the river itself. The view from directly opposite on the south bank is approximately on the line of Old London Bridge and remains one of the oldest and best views of the Monument. Most of the column of the Monument is visible from the south

bank walkway over the roof of St. Magnus the Martyr Church. It is important that there is no development north or south of the church which might harm this view.

4.2.5 For further information relating to Monument Views, please refer to the City of London Protected Views SPD 2012.

### **4.3 Mayor's London View Management Framework SPG: Protected Vistas**

4.3.1 The London Plan and the City of London Local Plan seek to protect strategic views which are defined within the London Plan's Supplementary Planning Guidance – the London View Management Framework (2012). In The Thames Policy Area, the following Protected Vistas need to be considered:

- Alexandra Palace to St. Paul's Cathedral (Wider Setting Consultation Area)
- Parliament Hill to St. Paul's Cathedral (Wider Setting Consultation Area)
- Kenwood to St. Paul's Cathedral (Wider Setting Consultation Area)
- Primrose Hill to St. Paul's Cathedral (Wider Setting Consultation Area)
- Greenwich Park to St. Paul's Cathedral (Viewing Corridor & Wider Setting Consultation Area)
- Blackheath Point to St. Paul's Cathedral (Viewing Corridor & Wider Setting Consultation Area)
- Westminster Pier to St. Paul's Cathedral (Viewing Corridor)
- King Henry's Mound, Richmond Park to St. Paul's Cathedral (Viewing Corridor & Wider Setting Consultation Area)
- The Queen's Walk to the Tower of London (Protected Silhouette, Viewing Corridor & Wider Setting Consultation Area)

4.3.2 Development proposals above Viewing Corridor threshold planes would normally be refused in accordance with London Plan policies 7.11 and 7.12 (as published July 2011). Development proposals in Wider Setting Consultation Areas must be designed or sited so that they preserve or enhance the viewer's ability to recognise and appreciate the Strategically Important Landmark, in this case St. Paul's Cathedral or the Tower of London. Further detail can be found in the London View Management Framework SPG.

### **4.4 Mayor's London View Management Framework SPG: River Prospects**

4.4.1 The London View Management Framework also includes a series of "River Prospect" assessment points from which the juxtaposition between elements, including the river frontage and landmarks, is appreciated within a wider London context. Development proposals must ensure that key features of each view can continue to be enjoyed in the context of their surrounding environment, including distant buildings. In the case of

the Thames Policy Area, the key landmarks include St. Paul's Cathedral, the Tower of London, the Monument, St. Bride's Church, Cannon Street Station towers and the Old Bailey.

#### **4.5 Views policy assessment requirements**

- 4.5.1 The process for assessing the impact of development on the protected views in the City is well established. Developers should use pre-application discussions to establish whether protected views might be affected by the development site. Where necessary developers should provide evidence with the application of how their development proposal responds to the protected views requirements in line with the City of London Protected Views Supplementary Planning Document.
- 4.5.2 The impact of vegetation and temporary structures on protected views is an important consideration. Planning conditions may be used to limit the potential for these elements to obscure protected views.

## 5 Recreation, public access and marine infrastructure

### 5.1 Recreation

5.1.1 The Thames Riverside is the City's largest public open space providing a traffic free environment for relaxation and recreation. The riverside varies in character along its length with areas suitable for sitting, walking, jogging and other sport and fitness related activities. Cycling and skateboarding are not allowed on the riverside walk. Multipurpose street furniture has been designed to provide opportunities for exercise and play promoting activities which assist in combatting both mental and physical health issues. Public toilets are provided in two locations on the riverside and participation in the Community Toilet Scheme adds to provision. The riverside's role in relaxation and stress reduction should be enhanced through the retention of areas which remain relatively tranquil compared with the vibrancy of other parts of the riverside and the City generally. The City of London Noise Strategy includes policies which will identify, protect and enhance areas of tranquillity. Planning decisions will contribute to the reinforcement of quiet areas.

### 5.2 Focal points of public activity

5.2.1 **Blackfriars Station** is one of the busiest stations in the City with recent Thameslink upgrade works increasing the number of passengers entering and exiting the station. Development Consent for the Thames Tideway Tunnel nationally significant infrastructure project was granted in September 2014 and will address the issue of London's combined sewer overflows into the River Thames. The development will result in the creation of a new area of public realm to the west of Blackfriars Bridge. This area will be subject to periodic closure to accommodate the maintenance requirements of the Thames Tideway Tunnel infrastructure. Blackfriars Pier will be moved to a new location east of the Blackfriars Bridge increasing the potential for transport interchange and public activity.

5.2.2 **Millennium Bridge** is one of the busiest pedestrian gateways into the City, with approximately five million people a year using the bridge, which provides a pedestrian link between the Tate Modern on the south side of the Thames and St Paul's Cathedral and the rest of the City. There is potential for improvement to the public realm on the Riverside Walkway beneath the bridge and surrounding area but any enhancements in vitality would need to respect the amenity of the adjacent City of London School and the nearby residential area.

5.2.3 Over eight million pedestrians per year use **London Bridge**. This level of footfall presents an opportunity to connect London Bridge with the Riverside Walkway and enhance vitality along the riverside. A new open staircase connecting the riverside and London Bridge will be constructed in 2015 and will replace the existing stairs.

5.2.4 **Tower Millennium Pier** is used by river bus services, river tour services and private-chartered boats. Due to its location just outside the City and adjacent to the Tower of London, a large number of tourists use this transport facility and there are opportunities to connect the pier with the

Riverside Walkway through public realm improvements at Three Quays and Sugar Quay. Passenger numbers are likely to increase in line with targets in the Mayor's River Action Plan

### **5.3 Riverside Walkway**

5.3.1 The riverside walk is a focal point of public activity and is popular amongst workers, residents and tourists for its relative tranquillity and views of the Thames. Riverfront redevelopment projects provide good opportunities to enhance the Riverside Walk and encourage a more vibrant and inclusive environment in places, as has successfully been done in the Angel Lane area.

5.3.2 The City Corporation has prepared a Riverside Walk Enhancement Strategy which identifies projects and proposals to improve the City's riverside walk in a co-ordinated manner, preserving the amenity of residents while identifying areas with potential for enlivenment.

5.3.3 The Riverside Walk Enhancement Strategy was subject to public consultation and Planning & Transportation Committee scrutiny during its preparation. It represents the City Corporation's intentions for the riverside walk and was adopted on 15<sup>th</sup> January 2015.

### **5.4 Inclusive design**

5.4.1 Development and riverside walk enhancements must be designed to ensure accessibility for people with a range of mobility and access needs. This includes provision of ramps or lifts adjacent to steps where there is a change in level between buildings and within the public realm, inclusive signage, tactile surfaces, consideration of soundscapes which aid orientation for people with visual impairments and the inclusion of riparian life-saving equipment. The Riverside Walk Enhancement Strategy identifies wheelchair accessibility as an issue for certain parts of the riverside and promotes improved accessibility associated with all public realm enhancement schemes. There is a wide range of improvements that can be made to assist in access and ease of use for people with a range of capabilities. Design and Access Statements which are submitted with development proposals should fully explore opportunities to provide a fully inclusive environment.

### **5.5 Access to the riverside and foreshore**

5.5.1 The City of London Corporation will continue to work with Transport for London, the Highway Authority for Upper and Lower Thames Street, on further improvements to existing pedestrian crossings and the introduction of new crossing facilities to further improve connectivity. The main points of access to and from the riverside from the rest of the City are at Victoria Embankment, Blackfriars Bridge, Millennium Bridge, Southwark Bridge, Cousin Lane, Angel Lane, London Bridge, Water Lane, Tower of London and Tower Pier. The number of people using each point varies by time of day and time of the week with the tourist attractions being more popular points of entry to the riverside at weekends. The popularity of the bridges

as entry points to the riverside necessitates that connections which are inclusive for all are provided from the bridges to the riverside walk.

5.5.2 Access to the foreshore, which is primarily in the ownership of the Port of London Authority, can have a recreational and educational value. There is a right of access for fishing, navigational and other customary purposes, but public access must be considered in the context of the environmental and archaeological importance of the foreshore and particularly safety considerations. Anyone going onto the Thames foreshore does so entirely at their own risk and should be aware of the following dangers: The Thames rises and falls by over 7 metres twice per day, the current is fast and the water is cold. Hazards include raw sewage, Weil's disease, broken glass, hypodermic needles, wash from vessels and slippery stairs.

5.5.3 There are steps down to the foreshore at the War Memorial on Victoria Embankment at Temple Gardens, Trig Lane, Cousin Lane, London Bridge and Custom House. Additionally there are private stairs at Vintners' Place and Fishmongers' Hall. These access points will be preserved. Ladders are provided at intervals along the frontage for emergency access and egress to and from the river and foreshore. These ladders should be retained and maintained. Life belts and grab chains are provided in several locations along the foreshore and on some of the bridges for emergency use.

5.5.4 . The use of a metal detector and associated digging on the Thames foreshore requires a permit from the Port of London Authority. Digging on the north foreshore between Lambeth and Tower Bridges, which include the city's stretch of the foreshore, is restricted to members of the Thames Mudlark Society. Digging is also prohibited in several sections including Queenhithe Dock (see figure 6 for restricted/prohibited digging areas). All finds must be reported to the Museum of London. The Thames Discovery Programme and Foreshore Recording and Observation Group coordinate events on the Thames Foreshore. The Museum of London coordinates archaeological recording

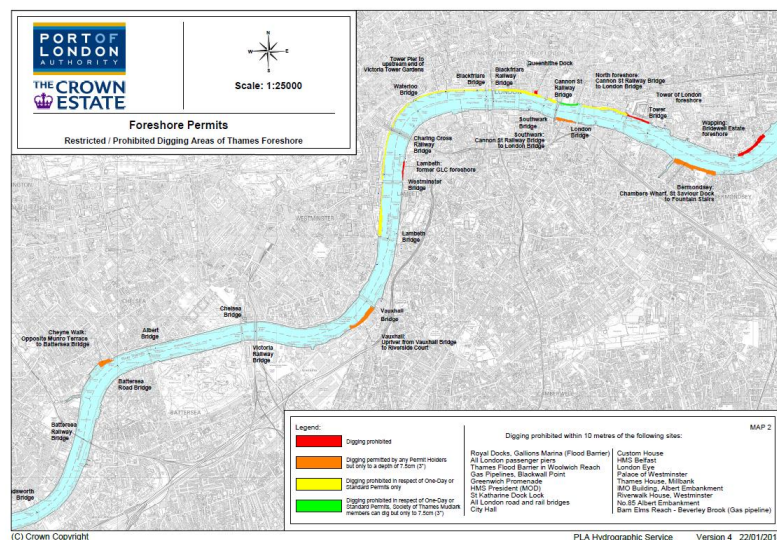


Figure 6 Permit requirements on the Thames Foreshore

## **5.6 Marine support facilities and infrastructure and moorings**

- 5.6.1 Passenger Transport nodes in the City at Blackfriars Pier, and just to the east of the City at Tower Millennium Pier, enable passenger movements to and from the north bank of the Thames. A redundant pier at Swan Lane could be brought back into use to improve access to river transport.
- 5.6.2 The River Thames in central London is well served with river cruises catering for tourists wishing to view London's sights from the river. River cruises alight at Blackfriars Pier in the City and Tower Millennium Pier just outside the City in Tower Hamlets to the east and also at Bankside on the south bank in Southwark. Numerous specialist cruises and party boats include the City's section of the Thames in their itineraries.
- 5.6.3 Walbrook Wharf (a safeguarded wharf) is currently used as a waste transfer station. Temporary pontoons elsewhere in the river within the City's boundary, are used on occasions to enable the transport of construction materials and waste by river from development sites (e.g. Blackfriars Station redevelopment and Thames Tideway Tunnel Infrastructure Project).
- 5.6.4 There is a private jetty at Sugar Quay and private stairs at Fishmongers' Hall and Vintners' Place which provide access to the river for small craft. Mooring points for larger leisure vessels are available at Tower Stairs Tier opposite HMS Belfast with access to the shore being via Tower Millennium Pier.
- 5.6.5 In order to ensure that the river can be used for its transport, navigation and recreation functions Local Plan policy CS9.4 (v) is of particular importance in refusing development on or over the river which does not require a waterside location for river related uses.

## **5.7 Access to flood defences**

- 5.7.1 The TE2100 plan promotes the ongoing maintenance of local flood defences in the City and elsewhere along the riverside with defence raising proposed from 2065. In order to carry out this maintenance work access to the flood defences from the land and water is essential. Improving access to flood defences can be achieved on redevelopment of riverside sites alongside public realm improvements. These works must be designed such that views of the river from footpaths and buildings are not affected when flood defences are raised. It may be necessary to set the line of the local flood defences back when the defences are upgraded, where space permits, to avoid erosion damage to the defences particularly at Blackfriars. Where realignment is not possible, local under-pinning of the defences may be needed.

## **5.8 River pageants and events**

- 5.8.1 Numerous boating events such as the Great River Race, the Doggett's Coat and Badge race, the Barge Driving Match and the Lord Mayor's Flotilla take place on the City's stretch of the Thames. In 2012, almost 1,000 vessels took part in the Queens Jubilee River Pageant. The Thames Riverside is important for accommodating supporters and spectators of

these events. Land based events such as the London Marathon which includes sections along Thames Street, the Lord Mayor's Show which runs along Victoria Embankment in the City and fireworks displays which focus on the river and its bridges also attract large numbers of spectators. It is likely that such events will continue to grow in popularity.

5.8.2 It is important that the Thames Riverside is designed to safely accommodate spectators and all potential visitors so that views of the river from the riverside and riverside buildings are not obscured. Policy CS9.2(i) and 9.4 (v) is of particular importance in maintaining such views of the river and refusing development on or over the river which does not require a river location. Policy CS9.4 (vi) is also important in restricting the permanent mooring of vessels to those which are of national historic interest and have a direct connection with the City.

## **5.9 Deficiencies in river related facilities**

5.9.1 The London Plan (paragraph 7.94) suggests that local authorities should identify deficiencies in the following river related facilities

- Water based passenger, tourism and freight transport
- Water based sport & leisure
- Access & safety provision
- Marine support facilities and infrastructure & moorings

5.9.2 The City Corporation has not identified any deficiencies in such facilities for the stretch of the Thames which passes through the City of London. This situation will be kept under review and support will be given to proposals for appropriate river related facilities.



## 6 Ecology, biodiversity, climate change & flood risk

### 6.1 Sites of ecological importance for nature conservation

- 6.1.1 The **River Thames** is a Site of Metropolitan Importance for Nature Conservation due to its role as a corridor for wildlife and importance as a fish spawning ground. The River Thames is now cleaner and sustains a greater diversity of wildlife than it has for 200 years, supporting 119 different species of fish. The designation of the River Thames as a SMINC demonstrates its importance as a migratory route and ecological corridor through the densely developed centre of London. The City's foreshore is also an important habitat for wildlife. Artificial structures, flood walls and littoral foreshore make it easier for natural flora and fauna to colonise. Birds such as cormorants and grey herons, which feed on invertebrates living in the inter-tidal mud, are found on the City's riverbank.
- 6.1.2 **Temple Gardens** is a Site of Borough Importance for Nature Conservation. The Temple Gardens are home to a variety of plants species both native and exotic. As well as the attractive horticulture, it is also home to a variety of wildlife, providing nesting sites for many birds, including robins, thrushes, coal-tits and blue-tits. Bees and a dozen species of butterfly can be spotted in summer.
- 6.1.3 **Seething Lane Garden** is a Site of Local Importance for Nature Conservation. This simple garden is important for its association with Samuel Pepys. The garden is undergoing redevelopment including excavation and reinstatement of a redesigned garden with improvements for all users as well as for wildlife, improving the garden's biodiversity and sustainability value.

### 6.2 Biodiversity Issues and Challenges

- 6.2.1 The Tidal Thames Habitat Action Plan which forms part of the City's Biodiversity Action Plan identifies a number of factors which must be taken into account when considering development on the riverside including maintenance of the existing foreshore, particularly areas where the foreshore is exposed at low tide providing intertidal habitat.
- 6.2.2 Vertical concrete flood defence walls, in some areas clad with timber, can support a variety of plants and invertebrates. The City Corporation encourages the use of timber in conjunction with the building of new structures, as timber walls are easier for natural flora and fauna to colonise.
- 6.2.3 Artificial structures in the channel, such as the pier structure in front of Customs House, can provide roosting and nesting sites for birds. Bird species which have been recorded in the City stretch of the Thames include: Cormorant, Mallard, Herring and Lesser black-backed gulls. Wherever possible, the City Corporation seeks to retain old timbers currently in use or redundant from past structures. All works in the river require a river works licence from the PLA and such works need to be kept in a good state of repair.

6.2.4 Climate change and increased sea levels may affect the amount of foreshore that is exposed in the future. The intensive level of use of the riverside is also seen as a threat necessitating the incorporation of biodiversity enhancement features to mitigate for any alteration in habitat due to development. Water quality is also identified as a threat although the proposed Thames Tideway Tunnel will alleviate the periodic discharge of sewage into the Thames which results from intense rain storms overloading the sewer system.

### **6.3 Climate Change mitigation and resilience.**

6.3.1 Recent developments on the riverside at Angel Lane demonstrate good energy efficiency measures and the use of renewables to reduce carbon emissions. However, although efforts to reduce carbon emissions are progressing, scientists generally agree that some level of climate change is now inevitable and we will need to adapt buildings and the public realm to ensure resilience to changed climate patterns.

6.3.2 The most likely scenario to impact on the City's riverside is one where summers will be hotter and drier, winters will be milder and wetter and there will be more frequent extreme weather events. The City's riverside with its south facing aspect will be particularly vulnerable to over- heating and potential drought conditions. Conversely, more frequent heavy rainfall will increase the likelihood of surface water flooding and sewer overflows with the Thames Riverside being one of the most vulnerable parts of the City. Maintaining the river and riverside as a biodiversity corridor presents challenges under changed climate conditions since flora and fauna may not adapt quickly enough to altered conditions. Changing weather patterns may encourage certain species to thrive and in the case of pests and diseases this could be detrimental to health and the environment. Resilient planting and selective use of lighting to prevent illumination of the water will contribute to the preservation of the biodiversity corridor. The river could also make a significant contribution to reducing air pollution through an increase in its use for commercial transport, which reduces lorry movements on congested streets.

6.3.3 Higher summer temperatures could increase the attractiveness of the riverside, increasing pressure on public spaces for recreation. The open south facing aspect can be utilised to aid solar gain to heat buildings in winter and facilitate green shading to reduce the need for air conditioning in summer. Greening can also provide shelter from cold winds, which are more prevalent on the riverside, reducing the use of outdoor space heating. The use of green infrastructure and Sustainable Drainage Systems (SuDS) is crucial in this area to maintain the biodiversity corridor and reduce rainwater run-off. SuDS designs in this area can make use of the proximity to the River Thames to enable clean rainwater to run directly into the Thames rather than into the combined sewer network.

### **6.4 Flood risk**

6.4.1 The City Flood Risk Area is defined in the Local Plan and shown on the Policies Map. Encompassing most of the Thames Policy Area, it comprises areas that are at risk from river flooding and surface water or sewer

flooding. Overall, the City of London is at low risk of flooding relative to other parts of central London. River flooding and surface water or sewer flooding present different challenges which need differing but complementary mitigation approaches. The Thames riverside is protected from river flooding by the Thames Barrier and wider flood alleviation mechanisms in the Thames Estuary, and locally by the flood defence walls adjacent to the river. Maintenance of these flood defence walls is essential. Therefore access to them from both land and river must be retained or improved, particularly on redevelopment of riverside sites. Climate change will result in rising sea levels which in turn will raise the river level in the tidal Thames. The City of London Strategic Flood Risk Assessment is available on the City of London's web site and provides detailed modelling of the flood risks the City faces.

6.4.2 The Thames Estuary 2100 (TE 2100) proposals in the City for the period from 2010 to 2040 focuses on maintenance of the existing flood defences. Beyond that date the TE2100 recommendation is to implement a programme of defence raising by up to 0.5 metres by 2065 and a further 0.5 metres by 2100, to protect central London from river flooding.

6.4.3 The City's riverside is also vulnerable to surface water flooding and overloading of the sewer network which results in sewer discharges into the River Thames at Blackfriars and the risk of dilute sewage emanating from manholes in the area. The catchment area for the City's sewer network extends well beyond the City encompassing much of the London Borough of Camden to the north and as far as Hammersmith and Fulham to the west. Intense or prolonged rainfall in these areas increases the risk of sewer discharges along Victoria Embankment and Paul's Walk on the City's Riverside. The recently consented Thames Tideway Tunnel will address London's combined sewer overflows into the River Thames

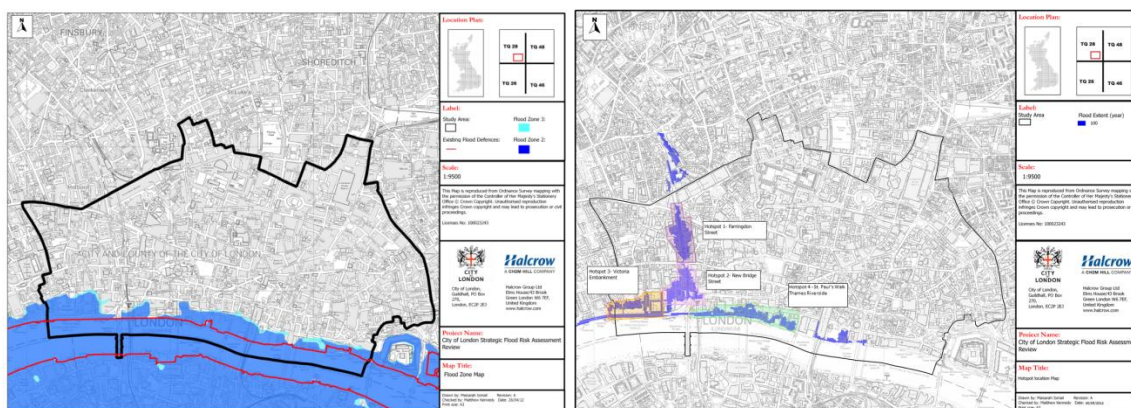


Figure 7 Flood Risk in the City

6.4.4 The increasing frequency of extreme weather events with climate change will increase the risk of surface water & sewer flooding in the future. It is important that development within the areas that are at risk should be designed with flood resistance and resilience in mind. Flood resistance should be designed to prevent the incursion of flood water into buildings through the use of impermeable structures at ground floor and basement levels, non-return valves on pipe and cable routes into the building and automated flood defence barriers at entrance points. Flood

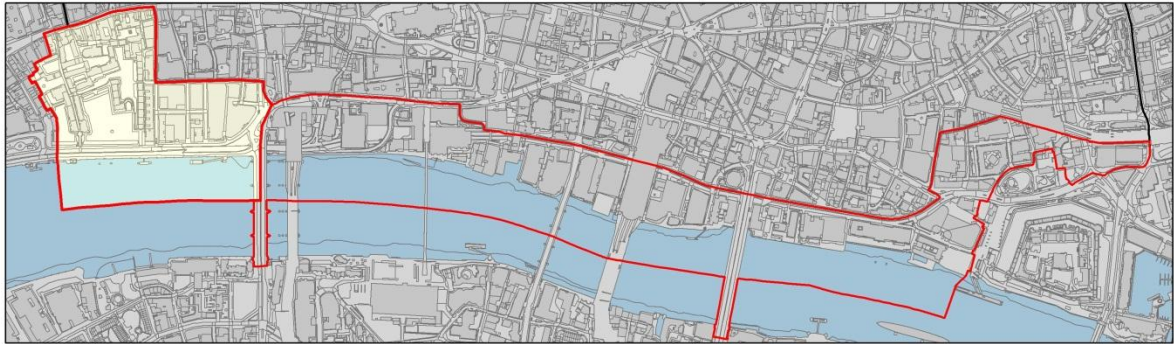
resilient designs ensure that, in the event of flooding, recovery times will be minimised by designing for ease of clean up and locating flood sensitive equipment above the projected flood water level.

- 6.4.5 The City of London Local Flood Risk Management Strategy addresses how the City Corporation as Lead Local Flood Authority is minimising the risk to businesses, residents and visitors from flooding.

## 7 Land use designations, issues and challenges

- 7.1 London Plan Policy 7.29 *The River Thames* proposes that Thames Strategies should be used to identify important riverside sites and determine the river related expectations for them. The following sections consider each part of the City's Thames Riverside and describe the character and heritage assets, issues and challenges affecting the area, planning policy intentions and development opportunities within each area.
- 7.2 In assessing each area the historic river related uses are highlighted and heritage assets such as Queenhithe dock are identified. River related issues and challenges include the safeguarding of Walbrook Wharf for waterborne freight uses and the development of the Thames Tideway Tunnel to tackle river pollution. For each area, a section on planning policy identifies the policy requirements for riverside sites set out in the Local Plan. River related challenges associated with development opportunity sites are highlighted.
- 7.2a Some issues apply to the whole of the Thames riverside within the City. For instance policy CS9.4(v) requires that proposed development on or over the river must require a riverside location for a river related use and CS9.4(vi) requires that permanently moored vessels must be of national importance , have a connection with the City and the River Thames, be used for a river related purpose and not have a detrimental impact on navigation, river regime or environment These requirements apply to all sections of the Thames Riverside. Similarly protection of the river's ecology and navigation must be considered in the design of lighting throughout the length of the City's riverside

## Temple Place to Blackfriars Bridge



### 7.3 Area character and heritage assets

7.3.1 The Thames Policy Area between Temple Place and Blackfriars Bridge is characterised by the Temples and Whitefriars Conservation Areas, Victoria Embankment and Blackfriars Bridge, all of which are heritage assets. This part of the City's riverside includes Blackfriars Pier but is dominated by the busy four lane road which runs along Victoria Embankment.

7.3.2 The Inner and Middle Temple legal precinct is a designated Park and Garden of Historic Interest and part of the Temples Conservation Area. The area contains a large number of listed buildings and structures. The collegiate plan creates an open aspect to the river with mature trees providing a screen along the Victoria Embankment. To the north and west chambers are arranged around an intricate variety of enclosed courts linked by intimate covered passageways.

7.3.3 Whitefriars Conservation Area contains large scale Victorian commercial buildings, several of which are Grade II listed, along with the former City of London School for Boys (Grade II). These buildings contribute to a formal grid street pattern, following the construction of Victoria Embankment which commenced in 1864 and was completed in 1870.

### 7.4 Issues and challenges:

- **Heritage assets** - conservation and enhancement of the character and appearance of heritage assets is a primary objective for this part of the Thames Policy Area.
- **Thames Tideway Tunnel** – this nationally significant infrastructure project is a major new sewer, needed to protect the tidal River Thames from increasing pollution. The project comprises a wastewater storage and transfer tunnel designed to intercept identified combined sewer overflows (CSOs), including one at Blackfriars, which frequently discharge into the tidal reaches of the River Thames. This will also create a new area of public realm, embanked from the Thames and located under and to the immediate west of Blackfriars Road Bridge. The existing Blackfriars Pier will be relocated to the east of Blackfriars

Bridge and the HMS President will be temporarily moved to the west. Disruption during construction works will present challenges in this area.

- **Garden Bridge.** Planning permission has been granted by Westminster City Council and the London Borough of Lambeth for a Garden Bridge to be built beyond the City's boundary to the west. This could result in greater pedestrian movement and the need for visitor amenities including toilets and refreshment venues. The proposed bridge will have potential impacts on protected views of St Paul's Cathedral, St Paul's Heights views, and protected views from the Monument. New views from the bridge could be created.
- The Mayor of London, as part of his Vision for Cycling in London is implementing **segregated cycle superhighways** to run east–west through the City between Tower Hill and Victoria Embankment and north-south between Farringdon and Blackfriars Bridge.

### 7.5 Planning Policy aims for Temple Place to Blackfriars Bridge

- 7.5.1 The Temples includes a mix of office and residential uses within the Chambers which creates a unique environment serving the needs of the legal profession. Change of use from office to residential in this area is acceptable provided that the balance between these two uses is maintained.
- 7.5.2 Elsewhere along this stretch of riverside, planning policy encourages office led commercial development. This could include further enlivening of the area around Blackfriars Bridge and the space adjacent to the riverside walk under the elevated section of roadway, with retail uses.
- 7.5.3 Development associated with the new sewerage infrastructure at Blackfriars will need to contribute to enlivening of the public realm and enhancement of the western section of the riverside walk in the City.

### 7.6 Relevant Local Plan policies:

- CS 9 Thames & the Riverside
- CS 2 Utilities Infrastructure
- CS 12 Historic Environment
- CS 13 Protected Views
- CS 18 Flood Risk
- CS 19 Open Spaces & Recreation
- DM 21.4 The Temples

### 7.7 Development Opportunities Temple Place to Blackfriars Bridge

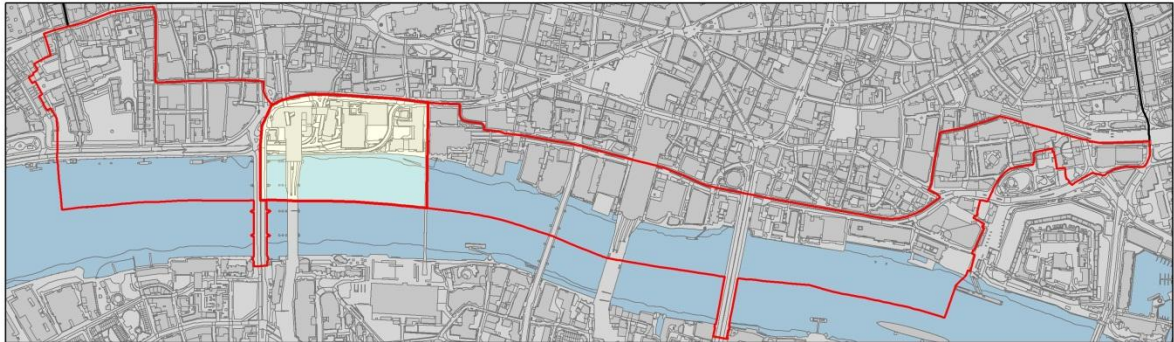
- 7.7.1 Although the Thames Tideway Tunnel site is an infrastructure site rather than a development site it presents opportunities for biodiversity enhancement, soft landscaping and recreation/play space in an accessible area of public realm on the riverside at Blackfriars. Prevention of flooding associated with this area is essential through the creation of flood defences that are suitable for the predicted river level rise due to climate change, and through the incorporation of sustainable drainage (SuDS) to reduce flood risk from surface water. The relocation of Blackfriars

Pier will create a transport hub with ease of transfer from rail to river services.

- 7.7.2 The proposed Garden Bridge, if built, will present challenges in dealing with additional pedestrian pressures and associated service needs at the City's western boundary



## Blackfriars Bridge to Millennium Bridge



### 7.8 Area character and heritage assets

7.8.1 The predominant character of the Thames Policy Area between Blackfriars Bridge and Millennium Bridge is post 1960s development which accommodates the entrance to Blackfriars underpass and the associated road network. Recently improved transport links at the redeveloped Blackfriars Station have increased passenger capacity. Brutalist concrete development dominates this area comprising the Mermaid Theatre and Conference Centre and Baynard House. Contrasting with this is the 1980s brick built City of London School and the glazed station building at Blackfriars Station. Blackfriars underpass accommodates heavy through traffic from Victoria Embankment through to Thames Street. Puddle Dock and White Lion Hill complete the complex road network in the area which effectively prevents pedestrian access to the riverside here from the rest of the City.

7.8.2 Blackfriars House was built during the period 1913-1916; this seven storey, steel framed commercial building is Grade II listed. Blackfriars Bridge (Grade II), the second bridge on this site, was opened by Queen Victoria in 1869 and widened on the western side between 1907 and 1910. The views to the west and east of the bridge are of major importance, as is the setting of St. Paul's Cathedral and several other landmarks such as those in the foreground on Victoria Embankment.

7.8.3 Baynard's Castle was originally built in 1087 on the west side of St Andrew's Hill, and destroyed to build the Blackfriars priory after 1275. A separate castle-like house called Baynard's Castle was built on the waterfront after about 1470. The foundations of the castle were exposed during archaeological investigations in the early 1970s and 1980s and have been preserved below modern buildings.

### 7.9 Issues and challenges

- The **severance of this area from the riverside walk** presents a challenge for any future redevelopment in the Baynard House/Puddle Dock area.
- The relocation of Blackfriars Pier to the east of Blackfriars Bridge will create **greater pedestrian pressure** on this section of the riverside walk. The relocated pier will be closer to the City of London School. It will be

important to prevent disturbance to the school. Relocation of the pier presents opportunities for increased use of the river pier and improved connections with Blackfriars Station and the rest of the City to the north. Additional visitor pressure could result from improved links between the proposed Garden Bridge and visitor attractions around the Millennium Bridge resulting in the need for an improved pedestrian environment.

- The presence of the City of London School on this stretch of the riverside must be taken into account in consideration of adjacent development, which must not disturb the **amenity of the school**.

## **7.10 Planning Policy aims for Blackfriars Bridge to Millennium Bridge**

7.10.1 Any redevelopment in this stretch should be focused on office led commercial development taking advantage of the improved rail and river transport links at Blackfriars and providing important employment premises to meet the needs of London and the wider south east. Improved pedestrian movement and the creation of additional open space could deliver future improvements in this area for businesses, residents and visitors. The preservation of Baynard Castle Scheduled Ancient Monument and public display and interpretation to enhance the public's understanding of its importance would be sought in any proposals for this area.

## **7.11 Relevant Local Plan policies**

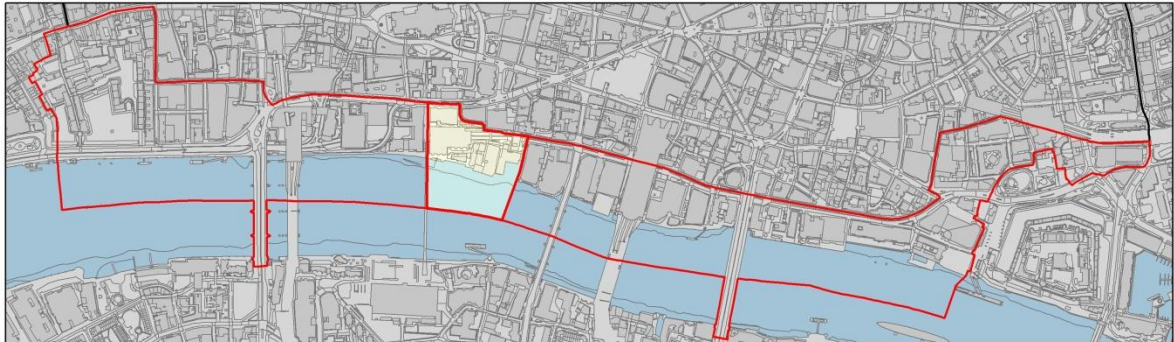
CS 9 Thames and the Riverside  
 CS 12 Historic Environment  
 CS 13 Protected Views  
 CS 18 Flood Risk  
 CS 19 Open Spaces & Recreation

## **7.12 Development Opportunities Blackfriars Bridge to Millennium Bridge**

7.12.1 The site at Puddle Dock and Baynard House is predominantly 1960s built concrete development incorporating offices and the former Mermaid Theatre which is currently used as conference facilities. In line with Local Plan Policy CS9 *Thames and the Riverside* any redevelopment should focus on office led commercial development which could incorporate conference facilities, hotel and retail uses as long as they do not prejudice the primary business function of the City (Policy DM 1.1). This site is constrained by the St Paul's Heights policies and Monument Views policies (CS13&14). The severance of the site from the riverside should be addressed in any redevelopment with additional publicly accessible open space being provided if possible. The Thames Tideway Tunnel scheme will provide replacement stairs and a new lift on the east side of Blackfriars Bridge.

7.12.2 Proposals for redevelopment will need to consider the role and future use of the local highway network in this area with the potential transfer of underused highways to open space use.

## Millennium Bridge to Queenhithe



### 7.13 Area character and heritage assets

7.13.1 The area around Millennium Bridge is dominated by the presence of the Millennium Bridge approach and the views of St Paul's Cathedral (Grade I listed) to the north. Buildings on the Millennium Bridge Approach date from the 1980s with the City of London School on the western side and buildings that are predominantly in office use, with cafes and restaurants at ground floor and basement levels on the eastern side. The City's main cluster of riverside residential properties sits to the east of Millennium Bridge comprising four residential blocks, Norfolk House, Globe View, Sir John Lyon House and Queens Quay with about 700 flats in total.

7.13.2 Queenhithe Scheduled Ancient Monument, known from 898AD is the only remaining Saxon and medieval dock on the City's riverside. At low tide a substantial area of foreshore and vertical flood defence wall is exposed forming important habitats for river related species. The riverside walk which is on the edge of the Thames for the majority of its path through the City is diverted onto Upper Thames Street in the Queenhithe area. Adjacent to Queenhithe Dock lies Smith's Wharf, constructed in the 19<sup>th</sup> century. Most of this Scheduled Ancient Monument lies underneath Queensbridge House.

### 7.14 Issues and challenges:

- Maintaining **residential amenity** is an important issue in this location particularly as the area's attractiveness to visitors is improved and the potential for noise disturbance increases.
- **Completion of the Riverside Walk at Queenhithe** Planning permission has been granted for completion of the riverside walk associated with redevelopment of adjacent buildings.
- The vulnerability of residents to **flood risk** is a key concern since this area is at risk from both river flooding and surface water/ sewer flooding. Flood resistance and resilience measures will play a role in protecting residents and enabling speedy return to normal following any flooding.

### 7.15 Planning Policy aims for Millennium Bridge to Queenhithe

7.15.1 In order to maintain residential amenity, entertainment venues, restaurants and bars should be located and designed to avoid conflict with the residential cluster. However increased visitor numbers are expected as tourist amenity is improved through environmental enhancement projects around the Millennium Bridge and Paul's Walk and the links to St Paul's Cathedral and the City Information Centre. This will lead to increased demand for cafes and restaurants. Planning and licensing conditions will be important in ensuring that any venues that are permitted do not result in undue disturbance to residents.

7.15.2 Aside from the small residential cluster at Queenhithe, the area should remain principally in office use, with office led commercial development being encouraged.

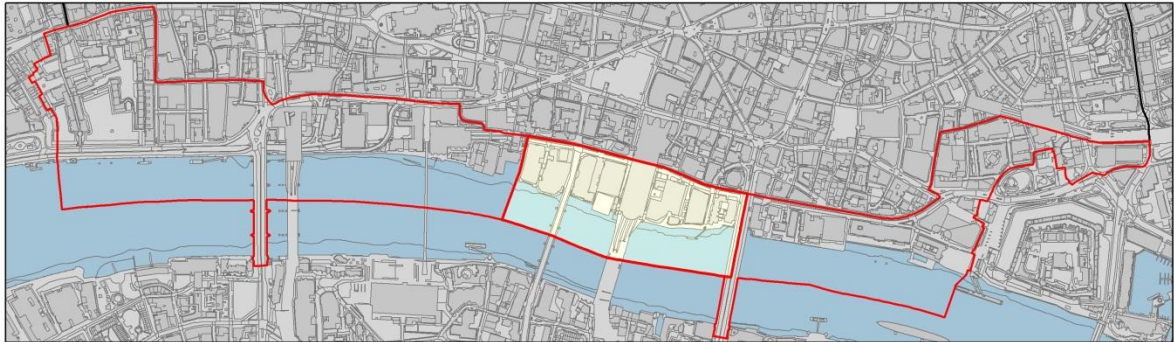
#### **7.16 Relevant Local Plan policies**

- CS 9 Thames and the Riverside
- CS 12 Historic Environment
- CS 13 Protected Views
- CS 18 Flood Risk
- CS 19 Open Spaces & Recreation

#### **7.17 Development opportunities Millennium Bridge to Queenhithe**

7.17.1 The residential cluster at this location limits opportunities for redevelopment although planning permission has been granted for the development of hotels at Queensbridge House (11/00572/FULMAJ) and Millennium Bridge House (12/00525/FULL) comprising over 500 bedrooms in total. Any further loss of offices in this area would need to satisfy the requirements of Policy DM 1.1 *Protection of Office Accommodation*. The Queensbridge House development includes the provision of a new section of Riverside Walk which will avoid the need to divert onto Thames Street for this section of the riverside walk. The main challenge for this area is maintaining residential amenity.

## Queenhithe to London Bridge



### 7.18 Area character and heritage assets

7.18.1 The predominant land use along this stretch of the Thames Policy Area is offices accommodated in a variety of buildings including modern, high quality office buildings: Watermark Place and Riverbank House. In contrast the historic Vintners' Hall and Fishmongers' Hall include the offices of City Livery Companies and provide event and conference facilities which service the business interests of the City and beyond.

7.18.2 The City's only remaining operational wharf is located adjacent to Cannon Street Railway Bridge and accommodates the City's Walbrook Wharf waste transfer station along with modern office space on the upper floors. Southwark Bridge, Cannon Street Railway Bridge and London Bridge make this a well-connected location for workers and visitors from a wide catchment.

7.18.3 This section of the Thames Policy Area includes numerous heritage assets including Southwark Bridge (Grade II listed), Vintners' Hall (Grade I listed) dating from 1671, Queen Street Place (Grade II listed), Cannon Street Station Towers (Grade II listed), Fishmongers' Hall (Grade II\* listed) and the Roman Governor's Palace site which is a Scheduled Ancient Monument.

### 7.19 Issues and challenges:

- The City's riverside has historically been a commercial area associated with the river – this commercial character remains today albeit in the form of offices rather than warehouses, wharves and docks. **Protection of office uses** will remain a key priority with supporting commercial uses such as cafes and restaurants being encouraged.
- **Servicing** is an issue for some buildings along the riverside with space being used intermittently for servicing and the requirement to service off Thames Street.
- Enhancing riverside **open space** is a key ambition with inclusive public access wherever it is possible, bearing in mind that the stretch of riverside walk from Queenhithe to Cousin Lane is rather narrow.

- At **Walbrook Wharf**, the retention of a viable wharf with river access and vessel manoeuvring space is essential.
- Pier infrastructure remains in place at **Swan Lane Pier** although the pier is no longer in use. Increased use of the river for passenger and freight transport would be welcomed.

## **7.20 Planning Policy aims for Queenhithe to London Bridge**

7.20.1 Office led commercial development which could incorporate café, restaurant or similar uses at ground floor level will be welcomed for its contribution to enlivening the riverside walk, within an overall approach which seeks to protect existing office space.

7.20.2 Walbrook Wharf presents particular policy issues since it is a safeguarded wharf and therefore protected for waterborne freight handling use (London Plan Policy 7.26). This policy also specifies that adjacent developments and uses should be designed to minimise potential conflicts. In addition Walbrook Wharf's use as a waste transfer or waste management site is protected. Any change of use to a non-waste related use would require re-provision of an equivalent waste site elsewhere.

7.20.3 If permanently moored vessels are exceptionally permitted they must be of national importance, have a special connection with the City and the River Thames, be used for a river related purpose and not have a detrimental impact on navigation, river regime or environment.

7.20.4 Reinstatement of the former Swan Lane Pier for a river related use would be welcomed.

## **7.21 Relevant Local Plan Policies**

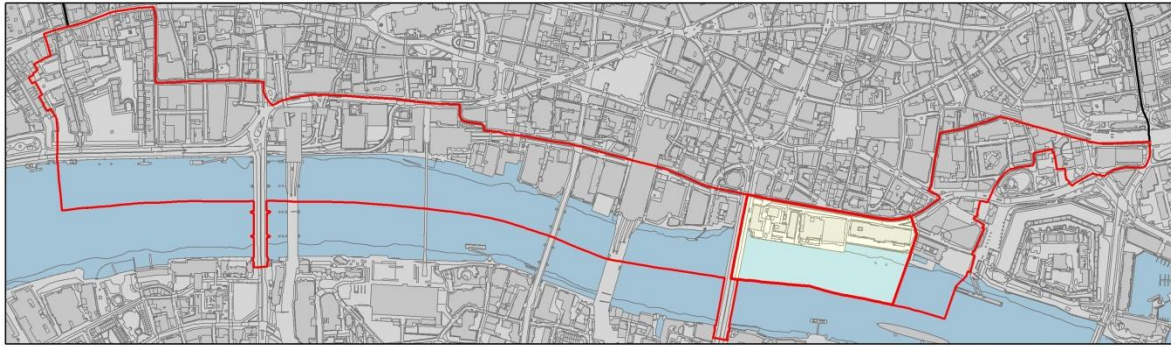
- CS 1 Offices
- CS 9 Thames and the Riverside
- CS 12 Historic Environment
- CS 13 Protected Views
- CS 17 Waste
- CS 18 Flood Risk
- CS 19 Open Spaces & Recreation

## **7.22 Development opportunities Queenhithe to London Bridge**

7.22.1 The established commercial nature of this stretch of the river will continue without dramatic change, though some upgrading of the existing office stock may occur. Re-use of the disused pier at Swan Lane for improved river transport would be welcomed.



## London Bridge to Water Lane



### 7.23 Area character and heritage assets

7.23.1 Heritage assets in this area include Adelaide House (Grade II listed), the Church of St Magnus the Martyr (Grade I listed), Old Billingsgate Market (Grade II listed) and Custom House (Grade I listed), and the Roman Bath House at 101 Lower Thames Street which is a Scheduled Ancient Monument. The 19th century Custom House houses HM Revenue & Customs offices at the eastern end of this section. Custom House Quay, the river wall, stairs and cranes are listed at Grade II\*. The Historic Billingsgate Market conference and event facilities service the business interests of the City and beyond. The Church of St Magnus the Martyr provides a place of worship occupying a site adjacent to the location of the old London Bridge.

### 7.24 Issues and challenges:

- This part of the riverside offers the potential to enhance the **City's attractiveness as a business location** through the retention of office led commercial development, utilising prime riverside office sites with iconic views of Tower Bridge and the River Thames. Office development, together with further retail, conference and event spaces, will provide increased vibrancy and footfall to this part of the riverside. These uses would not be compatible with the delivery of a high quality residential environment and therefore residential development along this part of the river will be discouraged.
- Identification of **suitable future uses for the historic buildings** in this area which conserve and enhance their historic significance and commercial heritage is a key challenge.
- The Riverside Walk Enhancement Strategy identifies opportunities for **public realm enhancement, improved public access for all and increased vibrancy** to complement the business City.
- The riverside walk in front of Custom House is too narrow and is liable to occasional **flooding**

### 7.25 Planning Policy aims for London Bridge to Water Lane

7.25.1 The primary planning aim for this stretch of the riverside is that it should provide office led commercial development with associated uses which

serve the needs of the business City. This could include retail and conference or event space which would not be compatible with residential development.

7.25.2 Better use of the riverward spaces adjacent to buildings would be welcomed. Car parking and vehicular access for servicing dominates several of the private spaces on this stretch of the riverside. Greater public access and more inclusive use of these spaces would be expected to complement any redevelopment. However some car parking may be needed to enable people with particular disabilities to park close to their destination.

7.25.3 Improved flood protection and the ability to raise flood defences in the face of climate change are important in this area.

## **7.26 Relevant Local Plan Policies**

- CS 1 Offices
- CS 9 Thames and the Riverside
- CS 12 Historic Environment
- CS13 Protected Views
- CS 18 Flood Risk
- CS19 Open Spaces and Recreation

## **7.27 Development opportunities London Bridge to Water Lane**

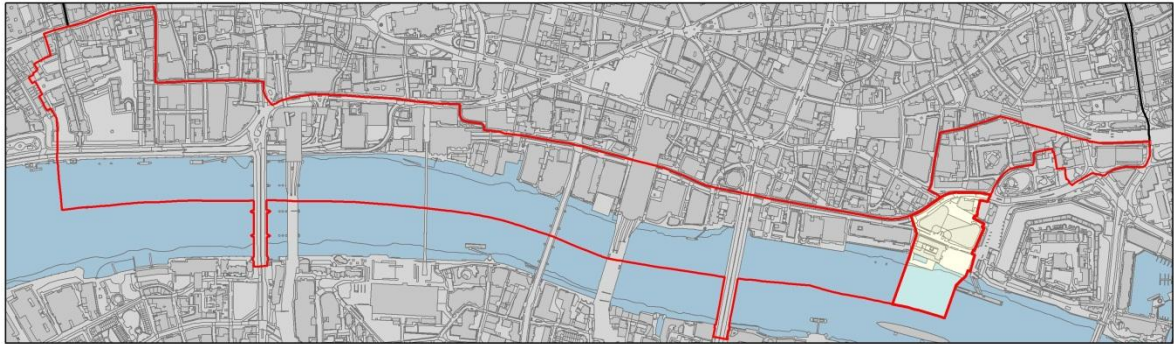
7.27.1 The area between the Church of St Magnus the Martyr and Old Billingsgate Market is currently in office use comprising two separate buildings: a concrete building with public viewing area adjacent to a blue glass clad building, both completed in the 1980s. The key issues for this area are access to the riverside and servicing for these and adjacent buildings to avoid the need for vehicles to load and unload on the riverward side of the buildings. The intention for this area is that it should be retained as office led commercial development realising the opportunity to enliven the riverside and provide pedestrian access and views of the river.

7.27.2 Future uses for the important listed buildings at Custom House and Billingsgate Market are a consideration. Billingsgate is currently in use as a conference and event space whilst Custom House is in office use. A range of commercial uses could be considered for these sites if the existing occupants vacate these premises. Alternative future uses should conserve or enhance the historic features of these buildings and should have river frontages that are well related to the adjacent open space and riverside walk.

7.27.3 Opportunities exist to improve the public realm on the riverside adjacent to historic Billingsgate Market and Custom House, particularly if the servicing arrangements and car parking issues can be resolved whilst recognising that some disabled people rely on the ability to park close to their destination.



## Water Lane to All Hallows by the Tower



### 7.28 Area character and heritage assets

7.28.1 New residential and hotel development characterises this part of the riverside with a new hotel and residential building at Three Quays and permission for residential development at Sugar Quay. Adjacent to Sugar Quay a jetty provides a private outdoor space over the river. The structure of the jetty and tidal range make the mooring of vessels here problematic. To the north of these riverside sites is a modern office complex at Tower Place and the church of All Hallows by the Tower. Three Quays and Tower Place are adjacent to the Tower of London World Heritage Site, Tower Millennium Pier and the City's boundary with Tower Hamlets.

7.28.2 All Hallows Church (Grade 1 listed) is of Anglo Saxon date, as witnessed by the arch in its tower which survives and although badly bombed in World War II, is medieval in outline. Its tower is of 1658.

### 7.29 Issues and Challenges:

- It will be important to provide and maintain **suitable public realm** adjacent to the new residential and hotel developments accommodating access to the buildings, circulation space for users of the Millennium Tower Pier, amenity space for visitors to the Tower of London and its environs, and spectator space for river and bridge based events. There is potential for improved integration of the Thames Path in the City with that in Tower Hamlets. Once Sugar Quay and Three Quays have been completed there will be little scope for further development in this area.
- **Sugar Quay jetty** is a privately managed open space comprising a wooden decked area over the river. This area has been used in the past for occasional charity events associated with the use of Sugar Quay as offices. Any future use permitted will need to incorporate a river related use and provide for public access to the jetty.
- The **Tower of London Local Setting Study** published by Historic Royal Palaces identifies important issues and considerations in protection and enhancement of the Tower's setting such as development in the Tower Setting Area and views of the Tower from surrounding streets.
- **Tower World Heritage Site Management Plan** was published by Historic Royal Palaces in 2007 and is currently under review. The purpose of the

Plan is to ensure the effective management of the World Heritage Site for present and future generations and to provide an agreed framework for long-term decision-making on the conservation and improvement of the Tower and its setting.

- **London's World Heritage Sites – Guidance on Settings SPG:** published by the Mayor of London in 2012, the SPG supports the implementation of London Plan Policy 7.10 (World Heritage Sites). It is further complemented by other London Plan Supplementary Planning Guidance as well as World Heritage Site management plans which have been prepared for each of the sites and which set out actions for safeguarding and enhancing their Outstanding Universal Value

### **7.30 Planning Policy Aims for Water Lane to All Hallows by the Tower**

7.30.1 The City Corporation recognises the importance of office development in this location as evident at Tower Place. Although Sugar Quay and Three Quays together would provide residential flats, this is not considered as a residential cluster. Subsequent to the granting of planning permission for these sites, the City's office protection policy has been strengthened. Further residential development in this vicinity will be resisted.

7.30.2 In line with the London Plan and City of London Local Plan policy, the use of the Sugar Quay jetty will be limited to uses which specifically require a riverside location for river related uses. Temporary mooring of vessels may be acceptable subject to health and safety concerns regarding safe access to vessels given the tidal nature of the Thames at this location. Permanent mooring of vessels will be resisted unless the policy criteria in CS9.4(vi) can be met. The location of the jetty adjacent to proposed residential development will further limit the use of this space due to residential amenity considerations.

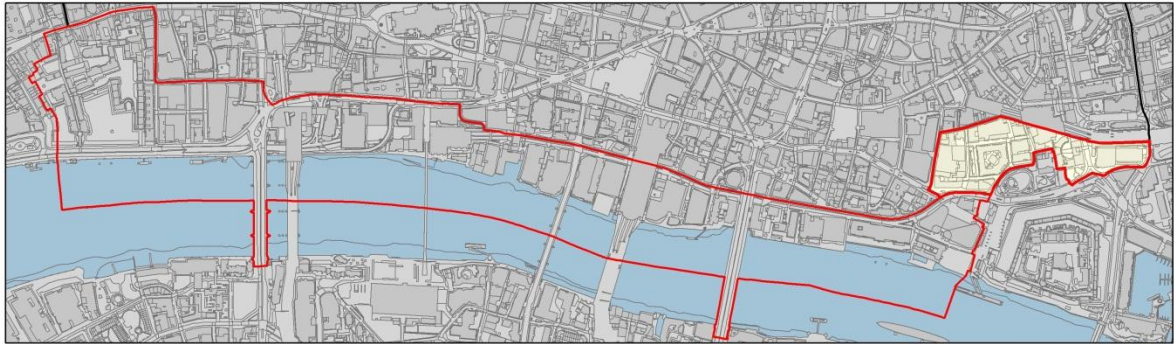
### **7.31 Relevant Local Plan policies**

- CS 9 Thames and the Riverside
- CS 12 Historic Environment
- CS 13 Protected Views
- CS 18 Flood Risk
- CS 19 Open Spaces & Recreation

### **7.32 Development opportunities Water Lane to All Hallows by the Tower**

7.32.1 There will be very little opportunity for development in this section of the riverside if the Sugar Quay permission is implemented, since the majority of sites within this section of the riverside have been developed recently.

## Mark Lane to Mansell Street



### 7.33 Area character and heritage assets

7.33.1 This section does not have a river frontage but is included as part of the Thames Policy Area because of its close relationship with the Tower of London which is an important landmark on the River Thames.

7.33.2 The City of London boundary cuts through the Tower of London Local Setting Area, with the Tower of London itself, Trinity Square Gardens and Tower Hill Station falling outside the City in the London Borough of Tower Hamlets. The Thames Policy Area extends north as far as Fenchurch Street Station and east to the City boundary at Mansell Street. This area includes the Trinity Square and Crescent Conservation Areas alongside modern hotels, residential and office blocks and Tower Gateway Station.

7.33.3 **Trinity Square Conservation Area** is focused on an intact group of mostly early 20th Century buildings with a distinctive imperial character unified by the use of Portland stone, all within the setting of the Tower of London World Heritage Site.

7.33.4 **Crescent Conservation Area** has an eighteenth-century street plan incorporating Crescent and Circus. When built it was one of the earliest planned developments in London and is one of the few such developments of this date to survive.

7.33.5 Other notable heritage assets in this area include a section of City wall which is a Scheduled Ancient Monument comprising one of the highest sections of medieval City wall above remains of the Roman Wall; St Olave's Church which is Grade I listed and dates back to the 15<sup>th</sup> Century; the former Port of London Authority building built in the 1920s and now being converted for use as a hotel and apartments, and Trinity House which was completed in 1796.

### 7.34 Issues and challenges

7.34.1 The **setting of the Tower of London** is an important consideration for this area. As a world heritage site protection is required for the area surrounding the Tower to protect views of the Tower from the local area but also to protect the backdrop of the Tower from further afield including river views (see paragraph 7.29)

7.34.2 Access to the riverside and to the Tower of London is separated from the rest of the City by the busy dual carriageway road which forms Byward Street and Tower Hill. This severance is a major issue in this area preventing the easy **movement of pedestrians** between Tower Hill Station and the Tower of London and the Thames riverside.

7.34.3 A cluster of **hotels** has emerged within the Mark Lane to Mansell Street area, serving the business City and tourist demand for close proximity to the Tower of London. It will be important to provide and maintain suitable public realm and way-finding within and adjoining the hotel cluster, accommodating access to the hotels and Tower Hill Underground Station and amenity space for visitors to the Tower of London.

### **7.35 Planning policy aims for Mark Lane to Mansell Street**

7.35.1 Local Plan Policy CS12 (5) seeks to enhance the Outstanding Universal Value, architectural and historic significance, authenticity and integrity of the Tower of London World Heritage Site and its local setting. Local Plan Policy CS13 (3) seeks to secure an appropriate setting of and backdrop to the Tower of London World Heritage Site, so ensuring its Outstanding Universal Value. The local setting of the Tower of London is shown on the Local Plan Policies Map A. Development proposals that affect the Tower of London should therefore assess their potential impact on the contribution made by its setting to its Outstanding Universal Value, through Environmental Impact Assessment including Townscape and Heritage Impact Assessments.

7.35.2 Any further development of hotels in this area must not compromise the business City. In line with policy CS11 Visitors, Arts and Culture and policy CS1 Offices evidence must be presented to demonstrate that further hotels are not compromising office development and will not create amenity issues for existing residential properties.

7.35.3 Although there are a number of small residential developments here the City Corporation does not consider this to be a residential cluster and therefore will resist further residential development in this area.

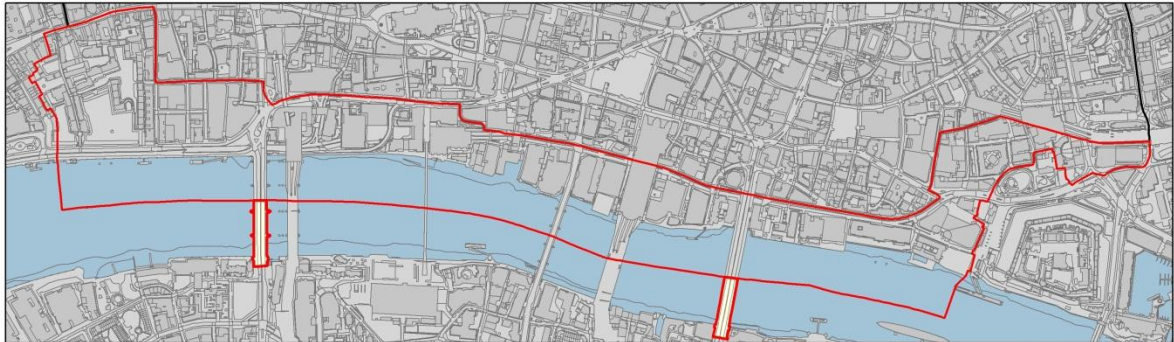
### **7.36 Relevant Local Plan policies**

- CS1 Offices
- CS 9 Thames and the Riverside
- CS11 Visitors Arts and Culture
- CS12 Historic Environment
- CS13 Protected Views

### **7.37 Development opportunities Mark Lane to Mansell Street**

7.37.1 The buildings on the corner of Byward Street at its junction with Mark Lane and the multi storey car park adjacent to Minories offer an opportunity for redevelopment. These sites are within the Thames Policy Area but are some distance from the river. However their proximity to the River Thames Site of Metropolitan Importance for Nature Conservation presents opportunities for biodiversity enhancement through provision of green roofs and walls.

## Thames Bridges and the Southern bridgeheads



### 7.38 Area character and heritage assets

7.38.1 The administrative boundary of the City includes the whole lengths of London Bridge and Blackfriars Bridge, including their bridgeheads on the south bank and the northern half of the other bridges. The current London Bridge was completed in 1973, and follows the alignment of the 19<sup>th</sup> century stone arched bridge formerly on this site. The original London Bridge, first built in Roman times, was located slightly further downstream. Blackfriars Bridge was first built in 1769; the current structure dates from 1869 and is Grade II Listed.

7.38.2 There are a number of listed structures associated with Blackfriars Bridge including the redundant bridge piers from the earlier bridge. Southwark Bridge is Grade II listed and Cannon Street Railway station towers are Grade II listed.

7.38.3 In the 1990s the City constructed a subway under the southern bridgehead of Blackfriars Bridge. This connected neighbouring sections of riverside walk along the south bank, which lie within the London Borough of Southwark. At London Bridge the south bank riverside walk follows Montague Close and Tooley Street, passing under the bridge approach south of the City boundary.

### 7.39 Issues and challenges

- Bridge House Estates is a charitable trust of which the City Corporation is the trustee, the primary object of which is the support and maintenance of Tower Bridge, London Bridge, Southwark Bridge, Millennium Bridge and Blackfriars Bridge. The trust's grant making arm The City Bridge Trust awards grants from charitable funds surplus to bridge requirements. Maintenance works on Blackfriars Bridge Southwark Bridge, and Tower Bridge must take account of their listed status/ statutory listed building controls.
- Any proposed development on the Thames Bridges must have a **river related use** and must not have a detrimental impact on navigation, river regime or environment and respect the river prospects and other protected views
- The Mayor of London's cultural strategy – "*achievements and next steps*" document commits the Mayor to exploring proposals for installing energy

efficient **lighting** schemes on a cluster of the Thames bridges in central London. This would need to be carried out sensitively to preserve and enhance the historic character of the bridges and other heritage assets and to avoid adverse impacts on the River Thames Site of Metropolitan Importance for Nature Conservation.

#### **7.40 Planning Policy Aims for the Thames Bridges and southern bridgeheads**

7.40.1 Current uses associated with the southern bridgeheads include the pedestrian underpass which includes an exhibition of pictures showing the construction of the bridge. At London Bridge, the John Rennie Vaults under the bridge head house a private leisure use. Any planning proposals for the City's bridges and the southern bridgeheads will be considered on a case by case basis

#### **7.41 Relevant Local Plan policies**

CS 9 Thames and the Riverside  
 CS12 Historic Environment  
 CS13 Protected Views  
 CS19 Open Spaces & recreation

#### **7.42 Development opportunities Southern bridgeheads**

7.42.1 There is very little opportunity for development on the City's Bridges or the Southern Bridgeheads.



## Screening Statement

On the determination of the need for a **Strategic Environmental Assessment** (SEA) in accordance with the *Environmental Assessment of Plans and Programmes Regulations 2004* and *European Directive 2001/42/EC* of the:

# Thames Strategy Supplementary Planning Document

## June 2015



**Sustainability Appraisal/Strategic Environmental Assessment Screening for:**

## Thames Strategy SPD

## 1 Purpose of Sustainability Appraisal (SA) / Strategic Environmental Assessment (SEA)

- 1.1 The SEA Directive identifies the purpose of SEA as “ to provide for a high level of protection of the environment and to contribute to the integration of environmental considerations into the preparation and adoption of plans and programmes with a view to promoting sustainable development” (Directive 2001/EC/42)
- 1.2 Sustainability Appraisal (SA) is the process by which this Directive is applied to Local Plan documents. SA aims to promote sustainable development through the integration of social, environmental and economic considerations into the preparation of plans.
- 1.3 The City’s Local Plan is subject to Sustainability Appraisal. However the 2008 Planning Act allows for Supplementary Planning Documents to be prepared without a full SA as long as they are screened to establish whether they will result in significant effects as defined by the SEA Directive.
- 1.4 The SEA Directive exempts plans and programmes from assessment “*When they determine the use of small areas at local level or are minor modifications to the above plans or programmes...*” and states that “*....they should be assessed only where Member States determine that they are likely to have significant effects on the environment.*”
- 1.5 The criteria for determining the significance of effects are taken from schedule 1 (9 (2) (a) and 10 (4) (a) of the Environmental Assessment of Plans and Programmes Regulations 2004 and are defined in appendix 1. These can be split into the criteria related to (i) the scope and influence of the document (ii) the type of impact and area likely to be affected

## 2 Purpose of the Thames Strategy SPD

- 2.1 The Key objectives of this SPD are:
- To provide guidance on development within the Thames Policy Area
  - To provide guidance on public realm enhancement within the Thames Policy Area
  - To explain the impacts on development of safeguarding of sites at Blackfriars and Walbrook Wharf
  - To promote river transport and provide guidance to assist implementation of increased river transport
  - To provide guidance on the heritage assets within the Thames Policy Area
  - To provide guidance on improving opportunities for biodiversity in the Thames Policy Area



- 2.2 This strategy is a Supplementary Planning Document which provides guidance regarding the City Corporation's Local Plan policies for the Thames Policy Area.
- 2.3 The London Plan and City of London Local Plan have been evaluated through the SA and Habitats Regulation Assessment (HRA) screening process, which incorporates the requirements of the SEA Directive, and have been found to be sound. This document provides details of how the City Corporation will apply the London Plan and Local Plan policies associated with the Thames Riverside within the City.

### 3 SEA Screening Procedure

- 3.1 The Responsible Authority (the City of London Corporation) must determine whether the plan or program under assessment is likely to have significant environmental effects. This assessment must be made taking account of the criteria set out in Schedule 1 of the Environmental Assessment of Plans and Programmes Regulations 2004 (see appendix 1), and in consultation with the Environment Agency, English Heritage and Natural England.
- 3.2 Where the Responsible Authority determines that the plan or programme is unlikely to have significant environmental effects, and therefore does not need to be subject to full Strategic Environmental Assessment, it must prepare a statement showing the reasons for this determination.
- 3.3 Appendix 1 shows the results of this screening process for the Thames Strategy SPD

#### 4 Screening and Consultation Outcome

- 4.1 This screening demonstrates that the Thames Strategy SPD is unlikely to have significant effects on the environment. Therefore it will not be necessary to carry out a full SA/SEA on this document.
- 4.2 Each of the statutory consultees has been consulted on this initial screening statement and their responses are summarised below:

Consultee	Response
Environment Agency	No SEA screening response received
Natural England	We agree that the Thames Strategy SPD is unlikely to have significant environmental effects and therefore that a full Sustainability Appraisal/SEA of it is not required.
Historic England	No SEA screening response received

- 5 Determination: The Thames Strategy SPD is unlikely to have significant effects on the wider environment since it provides guidance on the implementation of Local Plan policies which will have largely positive impacts. Therefore it will not be necessary to carry out a Strategic Environmental Assessment on this SPD.

## Appendix 1 Criteria for determining the likely significance of effects on the environment

SEA Directive Criteria Schedule 1 Environmental Assessment of Plans and Programmes Regulations 2004	Summary of significant effects
1. Characteristics of the Thames Strategy SPD having particular regard to:	
(a) The degree to which the SPD sets out a framework for projects and other activities, either with regard to the location, nature, size or operating conditions or by allocating resources	This SPD provides guidance on the application of policies which are defined in the Local Plan, regarding the City Corporation's Planning Policy aims for the area adjacent to the City's Riverside. The SPD will not set new policy or allocate resources.
(b) The degree to which the SPD influences other plans and programmes including those in a hierarchy	This SPD sits at the lowest level in the hierarchy of planning policy documents providing guidance on policies in the London Plan and the Local Plan. It may influence planning applications for sites on the riverside in line with the City Corporation's adopted policies for this area.
(c) The relevance of the SPD for the integration of environmental considerations in particular with a view to promoting sustainable development	This SPD provides guidance on the implementation of the City of London Local Plan. The Local Plan has been prepared in conformity with the NPPF's requirement for a presumption in favour of sustainable development and has been subject to Sustainability Appraisal.
(d) Environmental problems relevant to the SPD	Environmental problems which are relevant to the Thames Policy Area in the City: Biodiversity and water quality are potentially affected by the periodic discharge of dilute sewage into the Thames at Blackfriars during periods of heavy rain fall. The Thames Policy Area in common with the rest of Central London is an air quality management area for nitrogen dioxide and fine particulates (PM <sub>10</sub> ).
(e) The relevance of the SPD for the implementation of Community legislation on the environment (for example plans and programmes related to waste management or water protection)	The Air Quality Directive sets legally binding limits for concentrations of major air pollutants. The Water Framework Directive sets standards for water quality in water bodies such as the River Thames. This SPD supports London Plan and Local Plan policies which aim to improve air and water quality in line with the Air Quality Directive (2008/50/EC) and the Water Framework Directive (2000/60/EC)

SEA Directive criteria Schedule 1 Environmental Assessment of Plans and Programmes Regulations 2004	Summary of significant effects
2 Characteristics of the effects and area likely to be affected having particular regard to:	
(a)The probability, duration, frequency and reversibility of the effects	This SPD provides guidance on the implementation of Local Plan policies which will have generally positive impacts on the environment over the period of the Local Plan to 2026. Implementation of Local Plan policies according to this SPD is likely to result in incremental improvements to the environment in the Thames Policy Area.
(b)The cumulative nature of the effects of the SPD	The impact of this SPD in conjunction with other plans for this area such as the Riverside Walk Enhancement Strategy and the Tower of London World Heritage Site Management Plan will be positive for the environment.
(c)The trans boundary nature of the effects of the SPD	This SPD covers a small area at local level adjacent to the River Thames. The guidance in this SPD complements Local Plan policies which have been assessed through the Sustainability Appraisal (SA) and Habitats Regulation Appraisal (HRA) screening. These SA and HRA assessments concluded that Local Plan policies will not significantly impact beyond the City's boundaries. This SPD provides guidance on the implementation of Local Plan policies therefore it is unlikely to have any impacts beyond the City's boundary or any trans-national cross boundary effects.
(d)The risks to human health or the environment ( eg due to accident)	This SPD is unlikely to result in risks to human health.
(e)The magnitude and spatial extent of the effects (geographic area and size of the population likely to be affected) by the SPD	This SPD covers the Thames Policy Area and sets out guidance on the implementation of Local Plan policies which have been subject to Sustainability Appraisal and Habitats Regulation Assessment screening. This SA and HRA determined that the Local plan policies would have mainly positive impacts and any uncertain impacts would be mitigated by other policies in the plan.
(f)The value and vulnerability of the area likely to be affected by the SPD due to: <ul style="list-style-type: none"> <li>• Special natural characteristics or cultural heritage</li> <li>• Exceeded environmental quality standards or limit values</li> <li>• Intensive land use</li> </ul>	The River Thames is a Site of Metropolitan Importance for Nature Conservation (SMINC) and the Thames Policy Area which is covered by this plan includes several conservation areas, numerous listed buildings and the setting for the Tower of London World Heritage Site. The policies which this SPD supports aim to preserve and enhance these heritage assets. Water quality standards in the River Thames are breached periodically due to discharges

	<p>from the Combined Sewer Outflow at Blackfriars. The policies which this SPD supports aim to improve water quality through the implementation of the Thames Tideway Tunnel.</p> <p>Air Quality standards for nitrogen dioxide and fine particulates (PM10) are periodically exceeded in the Thames Policy Area. The policies which this SPD supports aim to reduce air pollution through the promotion of river transport.</p> <p>The Thames Policy Area in the City is an area of intensive land use with extreme pressure for development. The policies which this SPD supports aim to implement positive environmental effects associated with development.</p>
(g)The effects of the SPD on areas or landscapes which have recognised national Community or international protected status	<p>This area includes viewing corridors for the protected views of St Pauls Cathedral and the Tower of London. The policies which this SPD supports aim to safeguard these protected views.</p>

# **Equalities Impact Assessment**

## **City of London Thames Strategy Supplementary Planning Document**

**Draft for Adoption**

**June 2015**



Department of the Built Environment



**Equality Impact Assessment: Stage 1 Initial Assessment**

Service Area: Department of the Built Environment  
 Main Contact: Lisa Russell  
 Policy/Project/Service: City of London Thames Strategy Supplementary Planning Document Consultation Draft  
 Related Policies: The City of London Core Strategy 2011  
 (Please list) The Draft City of London Local Plan 2013

Is the function or policy carried out by a third party? Yes ☐ No ☒

To which of the following equalities issues could the Policy/Project/Service contribute? (Please tick as relevant)

1. Eliminating unlawful discrimination, harassment & victimisation & other unlawful conduct prohibited by the Act? 1 (Explain why in box below)  
 Yes ☐ No ☒
2. Advancing equality of opportunity between people who share a protected characteristic & those who don't? (explain how in box below)  
 Yes ☒ No ☐
3. Fostering good relations between people who share a protected characteristic & those who don't? (if possible identify the different groups and how relations could be improved in box below)  
 Yes ☐ No ☒
4. Is there any *potential* that this Policy/Project/Service could operate in a discriminatory manner?  
 Yes ☐ No ☒
5. Is there any evidence (including expressions of public concern or levels of complaints) of unequal impact or detriment to people sharing a protected characteristic or the undermining of good relations between people who share a protected characteristic & those who don't?  
 Yes ☐ No ☒
6. Based on the assessment just considered, what is the overall assessment of relevance (or risk) in terms of equalities?  
 Low ☒ Medium ☐ High ☐

Please provide brief details as above:

The Thames Strategy SPD Consultation Draft provides guidance on policies and sets out a strategy on the Thames Policy Area in the City. It lists five key objectives that seek to guide and protect development, protect heritage assets, promote biodiversity and consider flood risk and climate resilience. The implementation of the SPD will enhance open space and public realm which will encourage people

with protected and non-protected characteristics to use the space and thereby improve social cohesion.

Accessibility issues are addressed in a satisfactory manner in the SPD after input during the internal consultation stage which expanded the content of the document regarding inclusiveness.

The SPD includes sufficient references to relevant policy on inclusive design and will therefore have a positive contribution in terms of inclusive development in the Thames Policy Area.

Changes made to the consultation draft do not constitute major changes therefore there is no change to the findings of the Equality Impact Assessment Screening.

Completed by/date: Lisa Russell 18th September 2014

Manager Sign Off/date: Derek Read 18th September 2014

DBE Equalities Champion/date: Elisabeth Hannah 18th September 2014

1. The Act explains that having '*due regard for advancing equality*' involves:

- Removing or minimising disadvantages suffered by people due to their protected characteristics.
- Taking steps to meet the needs of people from protected groups where these are different from the needs of other people.
- Encouraging people from protected groups to participate in public life or in other activities where their participation is disproportionately low.

Policy/Project/Service where discretion may apply will also need to be considered when determining priority.

Please note that all new policies, projects or functions must be impact assessed as part of their development if any relevance to the duty is identified. The assessment should then be included as part of any decision or approval process e.g. included in Management or Committee reports.

Any requirement for new policies or restructuring of services and therefore the need to undertake an impact assessment, should be flagged-up well in advance as part of the annual service planning process to ensure it is part of the overall process and meaningful.



<b>Committee:</b> Planning and Transportation Committee	<b>Date:</b> 23 June 2015
<b>Subject:</b> Transport for London Grants for the 2015/16 Financial Year	<b>Public</b>
<b>Report of:</b> Director of the Built Environment Director of Markets and Consumer Protection	<b>For Decision</b>
<p style="text-align: center;"><b>Summary</b></p> <p>Transport for London will make two non-specific grants to the City in the 2015/16 financial year. These two grants total £1,024,000. They must be expended only on projects that serve to deliver the Mayor of London's Transport Strategy. This report proposes how these grants be allocated. All projects will be led on either by the Department of the Built Environment or the Department of Markets and Consumer Protection.</p> <p>This report also sets out the other grants that Transport for London will make to the City for specific purposes in the 2015/16 financial year that are known to date. These total £1,648,500. These grants cannot be used for any purposes other than those for which they have been made, for example, principal road maintenance, bridge strengthening and major schemes such as the Aldgate highway changes and public realm improvements project.</p> <p>The report also provides a summary of how the Transport for London grants, both specific and non-specific, for the City in the 2014/15 financial year were used.</p> <p><b>Recommendation</b></p> <p>The recommendation is that your Committee:</p> <ul style="list-style-type: none"> <li>• notes the grants that Transport for London will make to the City for specific purposes set out in table 1 to this report; and</li> <li>• approves the allocations of the non-specific grants set out in table 2 to this report and that these be submitted for approval by Transport for London.</li> </ul>	

## Main Report

### **Funding Empowerment**

1. Transport for London is empowered by section 159 of the Greater London Authority Act 1999 to give grants in respect of expenditure incurred by the City of London in doing anything that in the opinion of Transport for London is conducive to the provision of safe, integrated, efficient and economic transport facilities or services to, from or within Greater London.

## 2015/16 Specific Grants

- Under this provision, Transport for London will, in the 2015/16 financial year, make the specific grants to the City set out in table 1 *below*.

<b>Table 1: Specific Transport for London Grants in the 2015/16 Financial Year</b>	
<b>Project/Purpose</b>	<b>Grant</b>
<b>Major Schemes:</b>	
Aldgate Highway Changes and Public Realm Improvements Project	£1 400 000
<b>Infrastructure Maintenance:</b>	
Principal Road Maintenance	£134 000
Bridge Assessment and Strengthening Works	£28 000
<b>Skills:</b>	
Staff Training	to be determined
<b>Borough Cycling Programme projects:</b>	
Staffing (borough cycling programme)	£45 000
Cycle Parking	£25 000
Cycle Training Adults and Children	£12 500
Certificate of Professional Competence (CPC) Safer Urban Driver Training	£4 000

- Transport for London may possibly also make grants for one or more other major schemes (which are defined as schemes of a value of £1 million or more). The City has a number of applications in place for major schemes in addition to the Aldgate highway changes and public realm improvements project, these include Fleet Street enhancement, Bank junction and the Museum of London gyratory, and Transport for London may fund one or more of these major schemes in future financial years. The City may also receive funding for some other specific projects that are of particular importance to the Mayor of London or Transport for London (such as projects related to the Mayor's Vision for Cycling in London, e.g., cycle superhighways, cycle quietways or the cycle hire scheme).
- All of these grants can only be used for the specific projects and purposes for which Transport for London has made them. If the City is unable to expend the grant for that specific project or purpose this money will be lost to the City.
- In addition to specific grants, Transport for London will also make two non-specific grants to the City in the 2015/16 financial year. These are the Corridors, Neighbourhoods and Supporting Measures Grant, which is £924 000, and the Local Transport Funding Grant, which is £100 000. These two grants total £1.024 million (which is the same as the non-specific grant monies made available to the City by Transport for London in the 2014/15 financial year).

6. These two grants must be expended only on projects and for purposes that serve to deliver the Mayor of London's Transport Strategy. The only difference between the two non-specific grants relates to internal Transport for London administration. Transport for London assesses the City's spending plans for both of the non-specific grants and makes a determination that the City's proposals for use of the non-specific grants serve, in its opinion, to deliver the Mayor's Transport Strategy. The City has an excellent track record in this respect and Transport for London has never refused to approve any of the City's spending plans for the non-specific grants.
7. For the 2015/16 financial year, the recommendation is that the City allocates the £1.024 million of non-specific grant money from Transport for London as set out in table 2 *below*.

<b>Table 2: Proposed Allocation of the Non-Specific Transport for London Grants in the 2015/16 Financial Year</b>	
<b>Proposed Project</b>	<b>Proposed Allocation</b>
Bank Area Enhancement Strategy (Fishmongers' Hall Wharf ramp)	£279 000
Throgmorton Street/Bartholomew Lane (Austin Friars)	£196 060
City of London Pedestrian Model	£150 000
Bank Area Enhancement Strategy (Birchin Lane)	£82 500
Road Safety Education, Training and Publicity	£60 000
City of London Walkable World Cities Conference	£53 440
Road Safety Investigations	£50 000
Dropped Kerbs and Access Improvements	£40 000
City of London Freight Strategy	£30 000
Riverside Walk Enhancement Strategy (Glove View walkway)	£20 000
Nitrogen Dioxide (NO <sub>2</sub> ) Analysers	£20 000
Air Quality Measures Assessment	£20 000
20 mph Speed Limit Data Collection	£15 000
Dr Bike Cycle Maintenance Sessions	£8 000
<b>total</b>	<b>£1 024 000</b>

8. Table 3 to this report (located at the end for ease of reference) provides additional information on each of these projects, including its location, its proposed objectives and information about any previous consideration of the project by your committee(s).

### **2014/15 Specific and Non-Specific Grants**

9. Table 4 (located at the end for ease of reference) shows a summary of the projects delivered in 2014/15 from the £9.1m specific and non-specific grants and provides Members with a sense of scale and volume of work City officers have been responsible for delivering.

## Corporate and Strategic Implications

10. Expenditure of the specific and non-specific grants from Transport for London on the projects set out in tables 1 and 2 above would serve to deliver the relevant parts of the business plans of the Department of the Built Environment and the Department of Markets and Consumer Protection, which in turn would serve to deliver part of the Corporate Plan.

## Other Implications

11. Expenditure of the specific and non-specific grants from Transport for London on the projects set out in tables 1 and 2 above would serve to somewhat reduce the City's financial outgoings by making appropriate use of available external funding sources.

## Conclusion

12. Transport for London will make specific and non-specific grants to the City in the 2015/16 financial year. It is recommended that the City allocates the £1.024 million of non-specific grant money as set out in table 2 above.

## Appendices

none

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<b>Table 3: Proposed Allocation of the Non-Specific Transport for London Grants in the 2015/16 Financial Year</b>	
<b>Proposed Project/Project Details</b>	<b>Proposed Allocation</b>
<b>Riverside Walk Enhancement Strategy (Fishmongers' Hall Wharf ramp)</b>	<b>£279 000</b>
A high-priority project of the adopted Riverside Walk Enhancement Strategy. It will provide an accessible connexion at this currently inaccessible section of the Riverside Walkway. The project has committee approval at gateway 4 and approval at gateway 5 will be sought in July 2015.	
<b>Throgmorton Street/Bartholomew Lane (Austin Friars)</b>	<b>£196 060</b>
The Bank Area Enhancement Strategy identifies Austin Friars as one of its high priority projects, supporting the east-west movement of pedestrians along alternative routes to 'by-pass' Bank Junction. The enhancement works consist of re-paving the footways in Austin Friars, raising the carriageway to footway level, enhancing the soft landscaping, providing new seating and installing new energy efficient lighting.	

This proposed allocation was previously approved by Members in February 2015.	
<b>City of London Pedestrian Model</b>	<b>£150 000</b>
A computer model of pedestrian movement throughout the City. The model will aid with long-term planning of the City's streets and places and be used both as a development management tool to assist in evaluating the impacts of major new developments and a tool to identify areas of potential pedestrian road danger.	
<b>Bank Area Enhancement Strategy (Birchin Lane)</b>	<b>£82 500</b>
A high-priority project of the adopted Bank Area Enhancement Strategy. It will provide an improved and accessible walking route with timed access for vehicle servicing. The project has committee approval at gateway 5 and works are to start on site in June 2015. The project benefitted from Transport for London grant funding in the 2014/15 financial year.	
<b>Road Safety Education, Training and Publicity</b>	<b>£60 000</b>
We have a statutory duty (Road Traffic Act 1988, section 39) to carry out road safety education, training and publicity. A programme of road safety education to all City schools will be delivered over 2015/16, including pedestrian and cycle training. A programme of road safety publicity campaigns and other awareness interventions to vulnerable road users, City businesses, residents will be implemented over 2015/16.	
<b>City of London Walkable World Cities Conference</b>	<b>£53 440</b>
A City of London-hosted conference on walkability and the quality of the pedestrian experience, probably including international comparisons along with London. It may involve corporate and voluntary sector partners.	
<b>Road Safety Investigations</b>	<b>£50 000</b>
Investigating locations of high injury collision rates across the City and/or implementing measures to improve road safety. This is directly link to the Road Danger Reduction Plan.	
<b>Dropped Kerbs and Access Improvements</b>	<b>£40 000</b>
There are many locations across the City which is not fully accessible to pedestrians, in particularly those with mobility impairments. Measures such as new dropped kerbs, adjustments to existing ones, removal of obstruction, new crossings and other minor measures will be implemented to improve the accessibility of the City's streets.	
<b>City of London Freight Strategy</b>	<b>£30 000</b>
Identification of options for managing deliveries and freight activities in collaboration with City businesses in order to reduce the impact of heavy vehicles on the City's streets during the periods when pedestrian and cyclist flows are at their highest. Measures such as consolidation centres and timed-delivery areas will be investigated. The results of this work will feed into the City's freight strategy.	
<b>Riverside Walk Enhancement Strategy (Globe View walkway)</b>	<b>£20 000</b>
A high-priority project of the adopted Riverside Walk Enhancement Strategy. It will complete and open up the Riverside Walk at Globe View. Works are to be coordinated with the adjacent hotel development. The project has committee approval at gateway 3. The project benefitted from Transport for London grant funding in the 2014/15 financial year. The funding in the 2015/16 financial year is required to develop the design and to consult neighbours. This funding will enable the project to be taken to Gateway 4.	
<b>Nitrogen Dioxide (NO<sub>2</sub>) Analysers</b>	<b>£20 000</b>
The purchase of small portable analysers that measure the concentration of NO <sub>2</sub> in the air. The analysers are battery operated and can be fixed to posts so they can be	

<p>moved around the City to different locations as the need arises. The City is an air quality management area for NO<sub>2</sub>. The main source is motor vehicle traffic. The City Corporation has a statutory obligation to implement actions to reduce NO<sub>2</sub> concentrations as health-based targets are not being met. The analysers will be used to measure the effect on local NO<sub>2</sub> levels of traffic management and urban realm schemes. This will assess the impact of the particular measure and compliance with the legal limit values.</p>	
<b>Air Quality Measures Assessment</b>	<b>£20 000</b>
<p>Improving air quality in the City is the responsibility of both the Mayor of London and the City Corporation. The Mayor is implementing a range of measures such as the ultra-low emission zone, reducing emissions from the taxi fleet and cleaning up London's buses. The Draft Air Quality Strategy 2015 contains an action to assess what additional actions will be required, both locally and London wide, to meet the limit value for nitrogen dioxide (NO<sub>2</sub>) once the Mayor's schemes have been implemented. This work will be carried out by Policy Exchange, who will recommend a range of different scenarios that could be implemented to meet the air quality targets.</p>	
<b>20 mph Speed Limit Data Collection</b>	<b>£15 000</b>
<p>Collection of comprehensive City-wide traffic speed data to assess the effectiveness of the implementation of the 20 mph speed limit across the City and drivers' compliance with it.</p>	
<b>Dr Bike Cycle Maintenance Sessions</b>	<b>£8 000</b>
<p>Continuation of the successful free basic cycle maintenance sessions held fortnightly on the shared space on Queen Street to keep cyclists' bicycles roadworthy and keep City workers and residents cycling. Small mechanical faults are a significant reason for people stopping cycling or getting out of the habit of commuting by bicycle.</p>	

Table 4

#### SUMMARY TABLE OF 2014/15 FUNDED PROJECTS

#### TfL Summary Report for FY 14/15

		Allocation (£)	Full Year Value of Work Done (£)
<b>Borough Overall Total</b>		<b>9,135,000</b>	<b>9,134,260</b>
<b>Programme</b>	<b>Project</b>		
<b>Borough Cycling Programme (BCP) Total</b>		<b>191,500</b>	<b>191,500</b>
BCP CYCLE PARKING	BCP Cycle Parking - Installation of new cycle parking facilities both on-street and off-street	132,500	132,500
BCP CYCLE TRAINING ADULTS AND CHILDREN	Subsidisation of Bikeability training at levels 1, 2 and 3 to give City workers, residents and students the confidence to cycle in Central London conditions.	10,000	10,000

BCP CPC SAFER URBAN DRIVER TRAINING	Certificate of Professional Competence training of drivers in urban conditions to improve safety for vulnerable road users.	4,000	4,000
BCP STAFFING	Salary and on-costs for an additional member of staff to focus on delivering the City's Road Danger Reduction Programme.	45,000	45,000
<b>Borough Support - Staff Training Total</b>		<b>19,000</b>	<b>19,000</b>
CITY OF LONDON STAFF TRAINING FY 14/15	Various short-duration training courses for members of City staff and sponsorship for a longer degree course (MSc in transport planning).	19,000	19,000
<b>Bridge Assessment &amp; Strengthening Total</b>		<b>50,000</b>	<b>50,000</b>
NOBEL STREET RETAINING WALL	Nobel Street Retaining Wall	30,000	30,000
ST BOTOLPHS STREET SUBWAY	St Botolphs Street subway	20,000	20,000
<b>Central London Cycling Grid Total</b>		<b>207,000</b>	<b>207,000</b>
CENTRAL LONDON GRID SEED FUNDING	Staff time on developing the City's response to the <i>Mayor's Vision for Cycling in London</i> , particularly in designing the Central London grid in the City.	20,000	20,000
ROUTE 4 - WEST SMITHFIELD & BARBICAN	Cycling grid: Route 4—West Smithfield and Barbican.	93,000	93,000
ROUTE 6 - WOOD STREET	Cycling grid: Route 6—Wood Street.	20,000	20,000
WISLON STREET TO ALDGATE HIGH STREET	Cycling grid: Wilson Street to Aldgate High Street.	66,000	66,000
BYWARD STREET TO MIDDLESEX STREET	Cycling grid: Byward Street to Middlesex Street.	8,000	8,000
<b>Core Funding Total</b>		<b>150,000</b>	<b>150,000</b>
CENTRAL LONDON SUB-REGION CORE FUNDING	Central London Sub-Region transport partnership staffing, administration and projects.	150,000	150,000
<b>Corridors Total</b>		<b>971,500</b>	<b>970,760</b>
TWO-WAY CYCLING	Two Way Cycling - Installation of cycle infrastructure measures providing cyclists the opportunity to avoid busier and more dangerous streets. Cyclists can now cycle in the contra-flow direction in most of the City's one-way streets.	87,500	87,500
PEDESTRIAN MODEL	Scoping of a computer model of pedestrian flows around the City.	20,000	20,000
DROPPED KERB WORKS/INFORMAL CROSSINGS	Dropped Kerb Works/Informal Crossings - Footway drop kerb improvements to enhance carriageway crossing accessibility.	50,000	50,000
STREETS AS PLACES AND TRAFFIC MANAGEMENT	<i>Riverside Walk Enhancement Strategy:</i> Fishmongers' Hall Wharf ramp project development.	10,000	10,000
STREETS AS PLACES AND TRAFFIC MANAGEMENT	<i>Bank Area Enhancement Strategy:</i> Bank by-pass walking routes.	150,000	150,000
STREETS AS PLACES AND TRAFFIC MANAGEMENT	Scoping study of the potential for courtesy crossings of side streets across the City in order to reduce road dangers and enhance pedestrian priority.	136,000	136,000
STREETS AS PLACES AND TRAFFIC MANAGEMENT	Implementation of two courtesy crossings to reduce road dangers and enhance pedestrian priority.	50,000	50,000
CITY-WIDE 20 MPH SPEED LIMIT	Implementation of a City-wide 20 mph speed limit to reduce road traffic casualties, particularly for vulnerable road users.	178,000	178,000
CLFQP: KERBSIDE CONFLICTS PROJECT	Central London Freight Quality Partnership kerbside conflicts study (Kentish Town Road).	9,000	8,260

ST ANDREWS HOLBORN CHURCH GARDEN	Saint Andrew Holborn church garden enhancements and access improvements.	100,000	100,000
QUEENHITHE MOSAIC	<i>Riverside Walk Enhancement Strategy:</i> Queenhithe river wall mosaic subsidy.	50,000	50,000
MUSEUM OF LONDON/ST PAULS GYRATORY	Museum of London Gyratory - transformation of streets and urban realm by reintroducing two way traffic. Project spans a number of different programmes including Better Junctions, Cultural Hub urban realm and Concert Hall site	66,000	66,000
201 BISHOPSGATE PHASE III	201 Bishopsgate Ph 3 - Appold Street/Primrose Street Junction Improvements	36,000	36,000
LUDGATE HILL CROSSING	Light-controlled pedestrian crossing of Saint Paul's Churchyard at its junction with Ludgate Hill (zebra crossing conversion).	25,000	25,000
BILLINGSGATE MARKET RE-PAVING		4,000	4,000
<b>Local Transport Funding Total</b>		<b>100,000</b>	<b>100,000</b>
EDUCATION, TRAINING AND PUBLICITY	Travel Behaviour: Education, training and publicity. A programme of road safety education to all City schools will be delivered over 2015/16, including pedestrian and cycle training. A programme of road safety publicity campaigns and other awareness interventions to vulnerable road users, City businesses, residents will be implemented over 2015/16.	100,000	100,000
<b>Major Schemes Total</b>		<b>7,072,000</b>	<b>7,072,000</b>
ALDGATE GYRATORY	Aldgate Highway Changes & Public Realm Improvements - The Aldgate and Public Realm Improvements Project involves the introduction of two-way working on the Aldgate gyratory system and the creation of two new public spaces. The centrepiece of the scheme is a large public space between Sir John Cass's Foundation Primary School and St Botolph without Aldgate Church.	6,000,000	6,000,000
CCM APPLICATION SEED FUNDING	Development of potential interventions around the City's two Crossrail stations (Farringdon/Barbican and Moorgate/Liverpool Street).	12,000	12,000
BANK AREA STRATEGY	Scoping of options to improve Bank junction in order to reduce road dangers, particularly to vulnerable road users, and enhance the environment.	200,000	200,000
BANK AREA STRATEGY	Bank Courts & Lanes - Lombard St/Change Alley - Raised carriageway and new paving works	50,000	50,000
BANK AREA STRATEGY	Austin Friars - Raised carriageway and paving, new seating, lighting and way-finding	295,000	295,000
FLEET STREET AREA STRATEGY	Fleet Street Area Strategy Review - research and data collection and production of draft Strategy	64,000	64,000
FLEET STREET AREA STRATEGY	Fleet Street Corridor Study - research and data collection to inform design of the Fleet Street corridor	36,000	36,000
RIVERSIDE WALK AND FENCHURCH ST STRATEGY	Suffolk Lane/Laurence Pountney Hill - enhanced landscaping, paving and lighting	207,000	207,000
RIVERSIDE WALK AND FENCHURCH ST STRATEGY	Riverside Walk Enhancement Bell Wharf Lane - widened footways and new paving	143,000	143,000
RIVERSIDE WALK AND FENCHURCH ST STRATEGY	Riverside Walk Enhancement Blackfriars Bridge Walkway - development of concept designs for installation on riverside walk	25,000	25,000



RIVERSIDE WALK AND FENCHURCH ST STRATEGY	RWE:Globe View Walkway - structural investigations to inform design of enhanced riverside walkway	40,000	40,000
<b>Mayor's Air Quality Fund Total</b>		<b>144,000</b>	<b>144,000</b>
PILOT AIR QUALITY FOCUS AREA PROJECT	Pilot Air Quality Focus Area project	104,000	104,000
BARTS HEALTH NHS TRUST CLEANER AIR	Barts Health NHS Trust Cleaner Air Project	40,000	40,000
<b>Principal Road Renewal Total</b>		<b>230,000</b>	<b>230,000</b>
FLEET ST PRINCIPAL RD RENEWAL FY 14/15	Resurfacing of Fleet Street.	52,000	52,000
NEW FETTER LANE PRINCIPAL RD RENEWAL FY	Resurfacing of New Fetter Lane.	52,000	52,000
HOLBORN VIADUCT PRINCPL RD RENEWL FY14.15	Resurfacing of London Wall.	28,000	28,000
LONDON WALL PRINCIPAL RD RENEWL FY 14/15	Resurfacing of Holborn Viaduct.	55,000	55,000
CAMOMILE STREET PRINCIPAL ROAD RENEWAL	Resurfacing of Camomile Street.	43,000	43,000

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<b>Committee</b>	<b>Date</b>
Planning and Transportation	23 <sup>rd</sup> June 2015
<b>Subject:</b> City Development Trends Update: June 2015	<b>Public</b>
<b>Report of:</b> Director of the Built Environment & City Property Advisory Team	<b>For Information</b>

## Summary

This report is the first of a proposed six-monthly series providing an update on City development trends. It summarises recent information derived from our monitoring of planning permissions granted, setting it in the wider strategic and Local Plan context. It looks forward to developments likely to proceed in the next few years. It provides an overview of current market sentiment and highlights some key schemes and trends to note.

The overview as at June 2015 suggests that the City development industry is broadly on track to deliver the growth in floorspace to 2026 indicated in the Local Plan. Office, retail and housing growth completed since 2011 or under construction will contribute significantly towards meeting the Local Plan targets but further schemes will need to be commenced in future years for the targets to be met in full. Market sentiment is that office floorspace availability will tighten in the next few years leading to rental rises that trigger further development schemes.

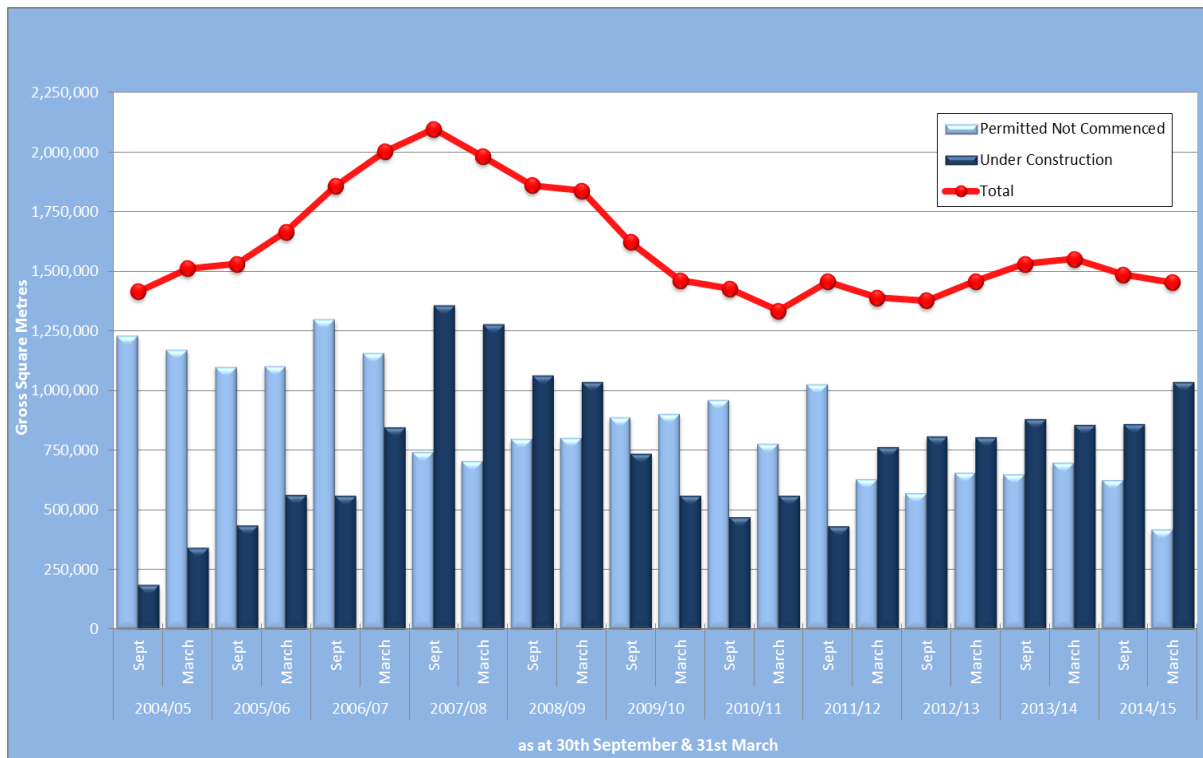
## Recommendations

It is recommended that Members note the content of this report.

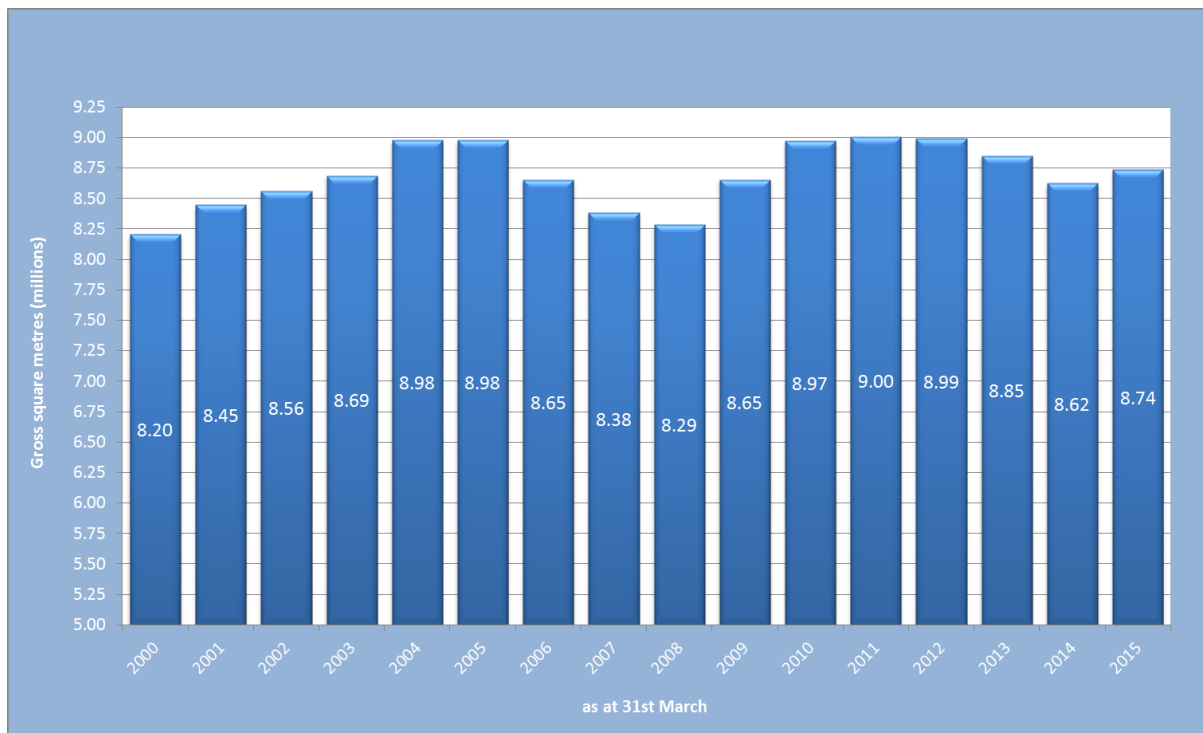
## Main Report

### Context: Current Development Overview

1. The six-monthly publication Development Monitoring in the City of London provides a regular snapshot of progress in delivering the development targets in the Local Plan. It is complemented by Development Schedules providing details on each planning permission and by topic monitoring and analysis papers all produced by the Monitoring and Information Team .
2. Total office floorspace under construction, 1,036,000 square metres gross (11,151,000 sqft), remains at its highest level since 2008 and it will deliver significant stock increase in the next three years.
3. Total office floorspace permitted not commenced, 417,400 sqm gross (4,493,000 sqft), remains below the 750,000 sqm gross (8,073,000 sqft) policy target in the Local Plan but this is in part due to the unusually high level of stock currently under construction.



City Office Floorspace Development Pipeline



City Office Floorspace Stock Estimate

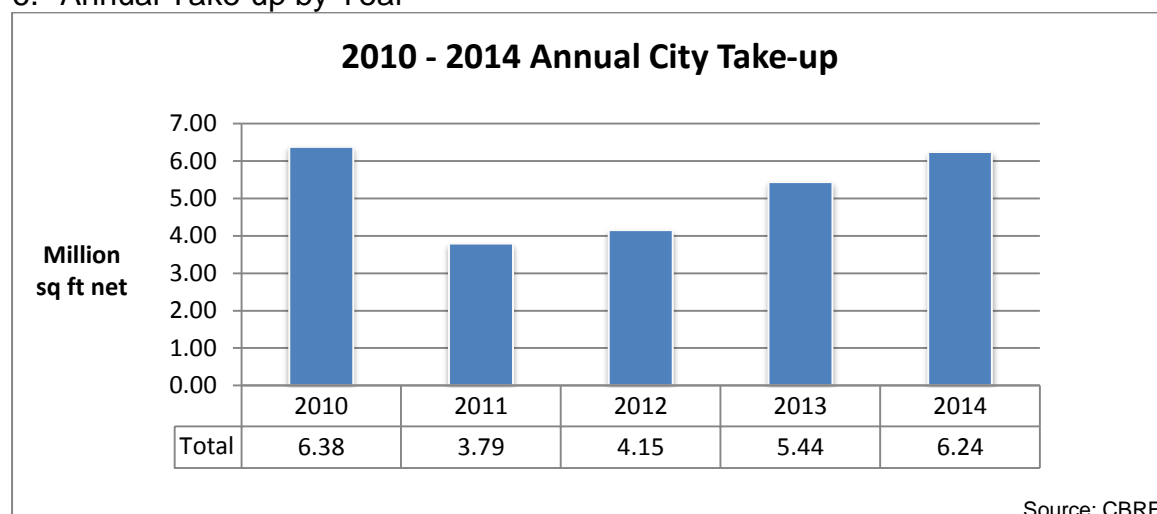
- Total office stock in the City was 8.74 million sqm gross (94.1million sqft) in March 2015. This has reduced by 0.26 million sqm gross (2.8 million sqft) since 2011 as sites have been cleared for redevelopment. Total stock will rise quickly as sites currently under construction are completed. It is estimated that stock currently under construction will contribute about 70% of the Local Plan office floorspace indicative growth target of 1.15 million sqm gross (12.4 million sqft)

required during 2011-2026. Therefore the City development industry is making good progress but further schemes will need to be permitted and implemented in the years ahead. The scale of current pre-application discussions suggests that there is an appetite among developers for additional office projects in the City.

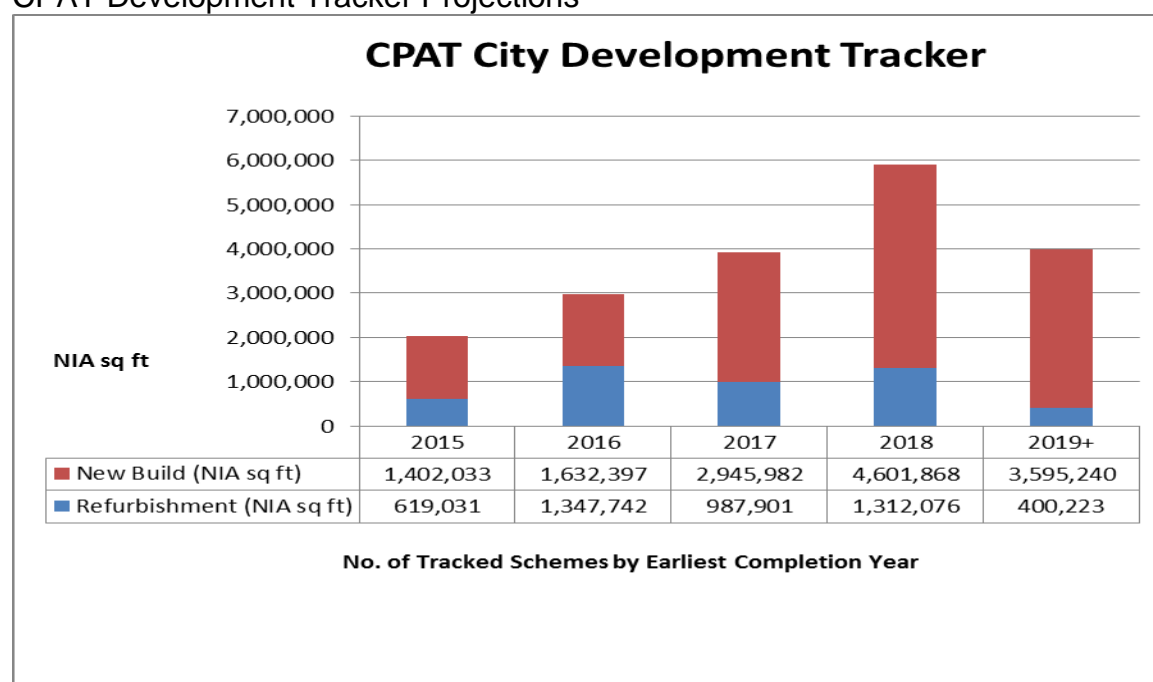
5. The City forms part of the wider London development market and economy. Strong growth projections for London as a whole and the trend for a wider range of occupiers to see the City as a good business location means that existing Local Plan indicative growth targets will be kept under review and will probably need to be exceeded.

## Context: Future Development Prospects

### 6. Annual Take-up by Year



### CPAT Development Tracker Projections



## Update: Market Sentiment

7. Figures are taken from trends reported by a range of City Agents. They are not directly comparable with planning permission monitoring data because they use different definitions of scheme commencement, include some City Fringe areas, and measure net floorspace which is typically 75% of gross floorspace.

### City Market Trends As at Q1 2015

Take-Up	12 months % change	Total for last 12 months
Increases (CBRE)	+12.82	6.24 million sq ft



Vacancy Rate	Q1 2014	Q1 2015
Decreases (KF)	8.40%	5.70%



Under Construction	12 months % change	Q1 2015
Increases (KF)	+7.90	5.08m sq ft (NIA)



Availability	12 months % change	Q1 2015
Decreases (KF)	-31.50	6.83 million sq ft



Investment	12 months % change	Total for last 12 months
Increases (CBRE)	+38.00	£11.3 billion



Active Requirements (million Sq ft)	12 months % change	Q1 2015
Increases (KF)	+12.60	4.46 million sq ft



Headline Rent (£)	12 months % change	Q1 2015
Increases (KF)	+7.70	£65.00 per sq ft



## **Update: Investment Turnover**

### **The Changing Leasing Market**

8. The City office market has been subject to significant change with some 46% of current stock completed during the past 20 years to provide for the changing needs of occupiers. Allied to the major regeneration of much of the City's office stock are the 600 listed buildings and many historic buildings within the City's conservation areas (36% of the City by area) that have contributed to maintaining a range of different types of stock to suit tenant's needs.
9. The leasing market has adapted over this period to provide significant flexibility so as to respond to tenant demands. In part this has been in response to practices established in other parts of Europe and the world, where for example French leases have been traditionally structured on a 3, 6 and 9 year basis. In the 1990s many of the significant lettings in the City were on the basis of 25 year 'upward only' institutional lease. Over time the nature many of the leases have shortened to provide for leases of 10, 15 and 20 year period with breaks built in to allow a degree of flexibility for both landlord and tenant. In shorter leases of 1-5 years it was also possible to insert break clauses although this has significantly reduced from 28% of leases to 2% between 2010 and 2014. This is evidence of the growing demand for short term space from occupiers, meaning that landlords are under less pressure to be flexible with incentives.
10. City businesses have been the beneficiary of the significant redevelopment activity in the Square Mile as a consequence of property owners letting space on short flexible terms to align with redevelopment aspiration for sites which often incorporate a number of buildings with different lease horizons. In addition to this flexible and affordable space often becomes available when large businesses take a decision to move in advance of their lease expiry and will look to sublet on short flexible terms for the remainder of the lease term, or where businesses no longer need all the space they have let (particularly prevalent in times of recession).
11. The type of offer available to smaller businesses has also evolved significantly over the last 10 years. Recent research undertaken by the City Corporation looked particularly at the changing serviced office provision in the Square Mile. Serviced offices provide centrally managed fully fitted and furnished flexible space. Between 2003 and 2012 the number of firms in serviced offices had grown from 365 to 1,600 housing 18,000 workers. The number of serviced offices has more than tripled over the last two decades from 25 to 85, comprising 200,000sqm of office space. Around 70% of the City's serviced office space is occupied by SME's, with the remainder providing representative offices or flexible space for larger corporates.
12. The continuing attractiveness of the City will be in ensuring that there are opportunities for businesses of all sizes to thrive with space that can attract new firms to the City, as well as providing flexibility and choice for those already located here.

13. In the future a broader range of offers will emerge. It is likely that serviced office providers will continue to differentiate their offerings in terms of quality and style with a stronger tiered market emerging. In addition to serviced offices there is a broader flexible space market emerging with managed 'co-working' providers starting to occupy space in the City. Moorgate Place at 1 Fore Street has recently completed a 170,000sqft letting to We Work who has also taken space at 9 Devonshire Square and 199 Bishopsgate. We Work provide a managed community of businesses with flexible space from sharing a hot desk at the lowest entry point to taking a number of desks in your own room. This type of operator provides a range of community spaces and will facilitate connections between businesses / individuals within the building. This is a step change in how the needs of new businesses are being accommodated in the Square Mile and will become increasingly prevalent over the coming years and will help accelerate the diversification of the City's business base.

### **Background Papers**

Development Monitoring in the City of London March 2015 at website link

<http://www.cityoflondon.gov.uk/services/environment-and-planning/planning/development-and-population-information/development/Pages/default.aspx>

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<b>Committee(s)</b>	<b>Dated:</b>
Planning & Transportation	23 June 2015
<b>Subject:</b> City Fund Highway Declaration – Hotel Development at 100 Minories EC3	<b>Public</b>
<b>Report of:</b> City Surveyor (CS.143/15)	<b>For Decision</b>

## Summary

Approval is sought to declare an area totalling 491 ft<sup>2</sup> of City Fund highway land at Tower Hill, EC3 to be surplus to highway requirements, to allow its disposal and enable the development which is being proposed by Grange Hotels Limited. Planning permission for the development was approved at your meeting of the 27 November 2012.

The permitted hotel development scheme at 100 Minories, EC3 is to have projections which oversail highway land above a fixed vertical Ordnance datum level.

Before third party interests can be granted in City Fund highway land, the affected areas first need to be declared surplus to highway requirements.

In this instance, the proposed development will oversail the highway stratum but will not impede it, thus stopping-up of the highway will not be necessary.

The terms for the highway disposal are to be reported separately for approval of the Property Investment Board subject to your approval to declare the affected area surplus to highway requirements.

## Recommendation(s)

Members are asked to:

- Resolve to declare an area of City Fund highway land at Tower Hill, EC3 measuring 491 ft<sup>2</sup> (45.61m<sup>2</sup>) and above 19.0m vertical Ordnance datum to be surplus to highway requirements to enable its disposal upon terms to be subject to the approval of the Property Investment Board.

## Main Report

### Background

1. Grange Hotels Limited (Grange Hotels) has obtained planning consent to construct a nine storey mixed use scheme at 100 Minories, EC3. The scheme is to have an overall gross area of approximately 203,000 ft<sup>2</sup> and comprising predominantly of hotel accommodation having 268 bedrooms each with an integral kitchen plus a ground level restaurant.
2. You approved planning consent (12/00263/FULMAJ) for the scheme at your meeting dated the 27 November 2012. The proposed development will have

projections from 2nd to 5th floors inclusive oversailing City Fund highway land at Tower Hill as identified on the attached plan.

### **Current Position**

3. Grange Hotels has approached the City Corporation seeking to acquire a suitable interest in the highway land affected by its approved development scheme.
4. Buildings where the construction and retention of habitable accommodation would be or is governed solely by a highway licence can be compromised as investments. Therefore for the purpose of promoting long term development, the City Corporation can dispose of suitable interests where permitted schemes encompass City Corporation owned highway land. The disposal of the highway land would assist works addressing the proper planning of the area.
5. Where City Fund highway is involved, before the City Corporation is able to dispose of any interests in it, the affected land must first be declared surplus to highway requirements.
6. Although the City Corporation can dispose of its highway land as a property owner, the highway stratum will nevertheless remain vested in the City Corporation as the highway authority until such time as it is stopped-up.
7. In this instance Grange is intending to oversail the highway, but will not impede it, thus stopping-up would not be required.
8. **Ordnance Datum Level** – The vertical measure used to derive spot geographic heights and taken as zero metres height at mean sea level at Newlyn, Cornwall which is the reference point for all height measurements.
9. **Affected Highway** - The area of City Fund highway land to be oversailed by the permitted scheme is situated at Tower Hill, EC3 and amounts to 491 ft<sup>2</sup> (45.61 m<sup>2</sup>) and lowest point of the built projection is to be 19.0m above Ordnance Datum.

### **Proposals**

10. Subject to your agreement to declare the area of City Fund highway surplus to requirements pursuant to S.233 Town & Country Planning Act 1990 and S.9 City of London (Various Powers) Act 1958, it is proposed that the City Corporation disposes of a suitable interest in the highway land upon terms to be approved by the Property Investment Board.

### **Corporate & Strategic Implications**

11. The disposal of highway land will support development and investment in the City, which inter alia ensures the supply of first class business accommodation in the City (A World Class City).

## Financial Implications

12. The financial implications of any disposal will be considered by the Property Investment Board.

## Legal Implications

13. **Stopping-up** – The proposed building projections will not encroach into the highway stratum, thus no stopping up of the highway is intended.
14. **Power of Disposal** - The transaction involves the disposal of City Fund highway land where part is held for planning purposes and part for highway purposes.
15. **Planning Purposes** - Disposal of land held for planning purposes is authorised by Section 233(a) & (b) of the Town and Country Planning Act 1990 and is expedient in order to secure the best use of land or to secure the carrying out of works for the proper planning of the area.
16. **Highway Purposes** - Disposal of land held for highway purposes is authorised by Section 9 City of London (Various Powers) Act 1958, which allows the City Corporation to dispose of its land within or outside of the City in such manner and for such consideration and on such terms and conditions as it thinks fit.

## Disposal

17. **Property Investment Board** - The terms of the negotiated highway disposal transaction are to be reported to the Property Investment Board for consideration, subject to you first declaring the affected City Fund highway land to be surplus to highway requirements.

## Conclusion

18. The necessary declaration confirming the highway to be surplus to requirements excluding the highway stratum, will enable development of the property according to the planning permission that has been granted for the 100 Minorities hotel scheme.

## Appendices

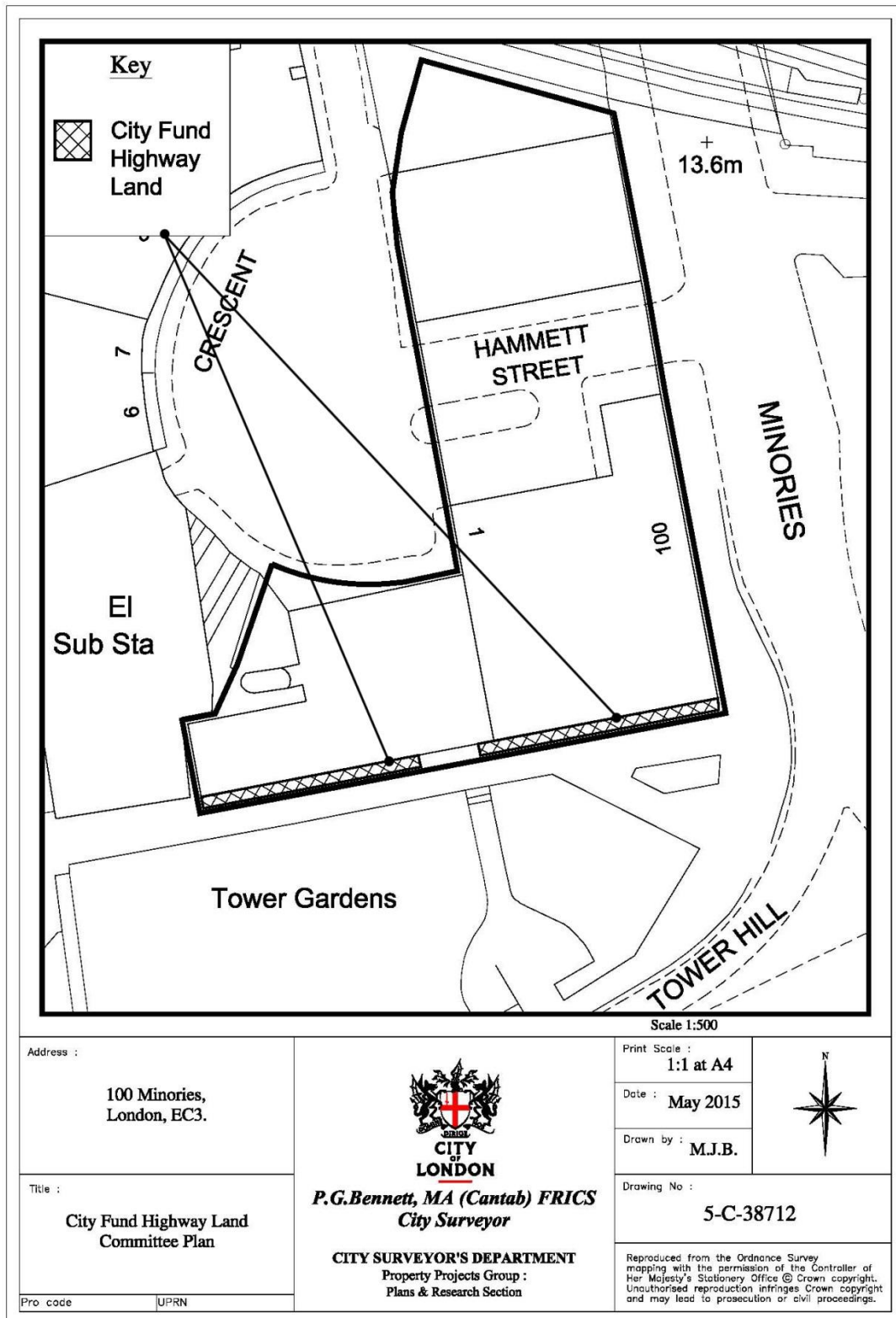
- Appendix 1 – Highway Plan
- Appendix 2 – Highway Vertical Section
- Appendix 3 – Scheme Images

## Background Papers:

Planning Consent no. 12/00263/FULMAJ.

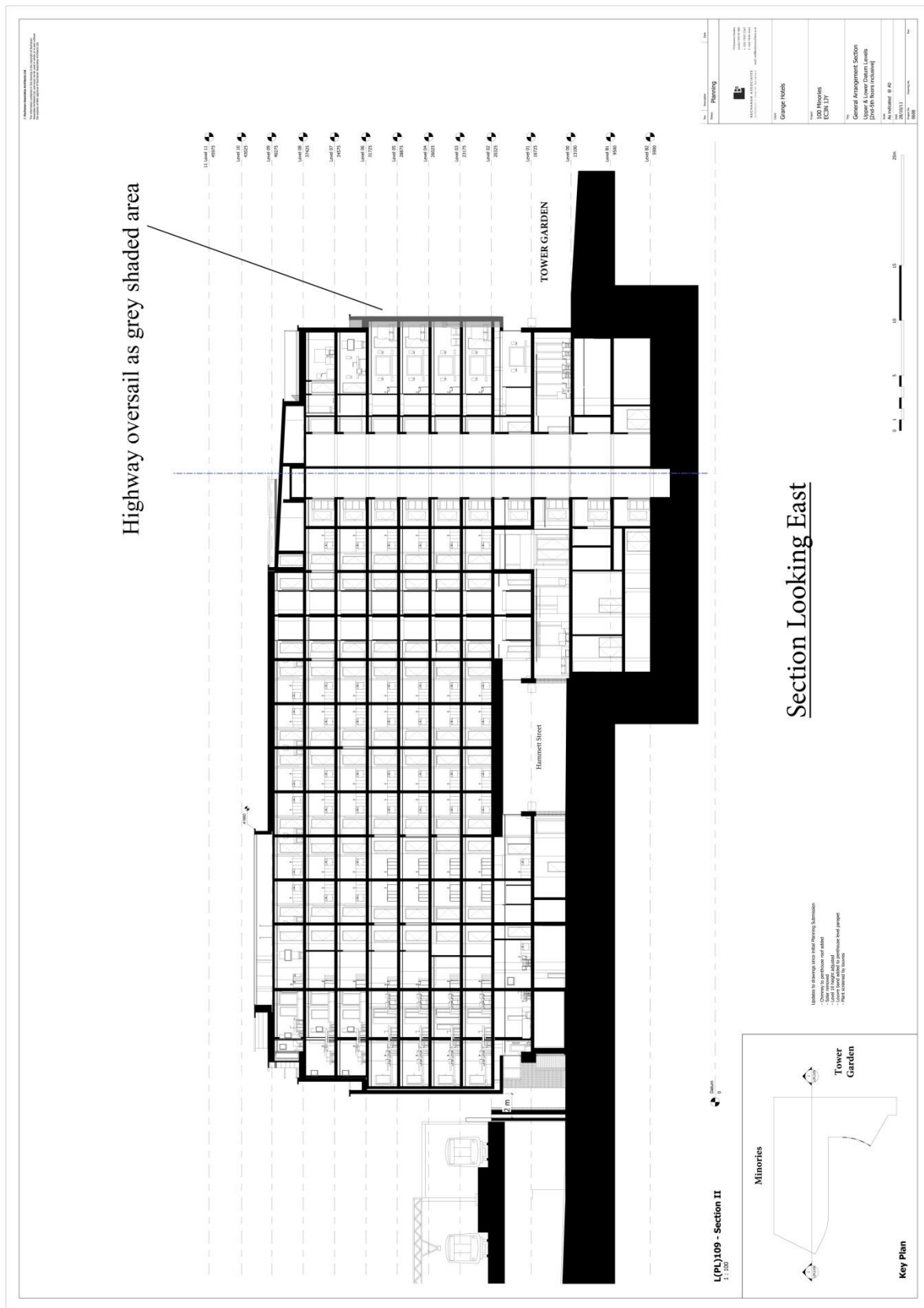
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# Appendix 1 – Highway Plan (100 Minories, EC3)



23 June 2015

## Appendix 2 – Highway Vertical Section (100 Minorities, EC3)



23 June 2015



### Appendix 3 – Scheme Images (100 Minorities, EC3)



23 June 2015

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<b>Committee:</b>	<b>Date:</b>
Planning and Transportation Committee	<b>23 June 2015</b>
<b>Subject:</b> Context and Update Sessions	<b>Public</b>
<b>Report of:</b> Town Clerk	<b>For Information</b>

## Summary

This report provides details of forthcoming Context and Update sessions for the Planning and Transportation Committee which will ensure that Members are fully equipped with the knowledge and tools necessary to undertake their role as a Member of the Committee.

## Recommendation

That the details of the sessions be noted.

## Main Report

1. The purpose of this programme is to enhance Member's understanding of the background and context to the work of the Committee, of its various impacts and of a number problematical and recurring technical aspects.
2. Responses to a survey indicated that Members preferred sessions to be held in the afternoon after Committee from 2pm to 3pm. There was little support for breakfast sessions; however, these do provide an opportunity for leaders of outside organisations to come to speak to the Committee on an informal basis and therefore one breakfast session has been scheduled in 2015.
3. The programme for 2015 is as follows –

Date	Time	Subject	Venue
2 June	12:30pm	The Considerate Contractor Scheme – Department of the Built Environment	Livery Hall
23 June	2pm – 3pm	Deconstruction and Construction Manual – Department of Markets and Consumer Protection	Livery Hall
15 September	8:30am – 9:30am	The Development Industry's Viewpoint - CPA	Private Members' Dining Room
27 October	2pm – 3pm	Sunlight and Daylight	Livery Hall
15 December	2pm – 3pm	What does CPAT do? – City Surveyor's	City Centre

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